

KEY PLAN  
NTS

DRAWING NO. DRAWING TITLE

PROJECT No. 218-2659  
WORK OF: GRADING,  
GRANULAR BASE, HOT MIX PAVING,  
CULVERT INSTALLATION

HIGHWAY No. 26  
REGION WEST  
LOCATION: AT LAKESHORE ROAD EAST  
LENGTH: 430.00m

ISSUED FOR CONSTRUCTION  
PROJECT No.: 218-2659

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This Drawing is For  
INFORMATION  
ONLY  
This Drawing should not be  
redistributed beyond the  
intended recipient

MASTER LEGEND	
EXISTING FEATURES (EX.)	
	EX. CONTOUR
	EX. GRADE
	EX. TREELINE
	EX. WATERCOURSE
	EX. DITCH
	EX. WATERMAIN
	EX. WATER SERVICE
	EX. FIRE HYDRANT & VALVE
	EX. SANITARY SEWER & MANHOLE
	EX. SANITARY FORCEMAIN
	EX. SANITARY SERVICE
	EX. STORM SEWER & MANHOLE
	EX. STORM CATCHBASIN
	EX. STORM DOUBLE CATCHBASIN
	EX. STORM CATCHBASIN MANHOLE
	EX. STORM DOUBLE CATCHBASIN MANHOLE
	EX. GAS MAIN
	EX. BELL LINE
	EX. BELL PEDESTAL
	EX. CABLE TELEVISION PEDESTAL
	EX. HYDRO POLE
	EX. LIGHT STANDARD
	EX. SIGN
	EX. BUILDING
	EX. BENCHMARK NUMBER & LOCATION
	EX. BOREHOLE NUMBER & LOCATION
PROPOSED FEATURES (PR.)	
	PR. PROPERTY LIMITS
	PR. ELEVATION
	PR. ELEVATION (MATCH EX. ELEVATION)
	PR. SWALE & SLOPE
	PR. DITCH DRAINAGE
	PR. WATERMAIN & VALVE
	PR. WATER SERVICE
	PR. FIRE HYDRANT & VALVE
	PR. WATER VALVE CHAMBER
	PR. WATER QUALITY TESTING STATION
	PR. SANITARY SEWER & MANHOLE
	PR. SANITARY FORCEMAIN
	PR. SANITARY SERVICE
	PR. SANITARY CATCHMENT
	PR. CATCHMENT AREA ID
	PR. AREA (ha)
	PR. POPULATION (2.7 p.p.u.)
	PR. STORM SEWER & MANHOLE
	PR. CATCHBASIN
	PR. DOUBLE CATCHBASIN
	PR. CATCHBASIN MANHOLE
	PR. DOUBLE CATCHBASIN MANHOLE
	PR. FOUNDATION DRAIN SERVICE
	PR. STORM CATCHMENT
	PR. CATCHMENT AREA ID
	PR. RUNOFF COEFFICIENT
	PR. DRAINAGE AREA (ha)
	PR. CURB CUT
	PR. CANADA POST COMMUNITY MAIL BOX
	PR. TRANSFORMER
	PR. STOP SIGN
	PR. NAME SIGN
	PR. NO PARKING SIGN
	PR. FENCE
	PR. BUILDING ENVELOPE
	PR. LIGHT DUTY SILT FENCE
	PR. HEAVY DUTY SILT FENCE
	PR. STRAW BALE CHECK FLOW
	PR. ROCK CHECK DAM
	PR. SLOPE (3:1 MAX.)
	PR. TREE PRESERVATION AREA
	PR. TOPSOIL STOCKPILE LOCATION
	PR. PHASING LIMIT



Ministry of  
Transportation



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CONSTRUCTION NOTES:

A) GENERAL

- ALL WORKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH MINISTRY OF TRANSPORTATION (MTO), ONTARIO PROVINCIAL STANDARDS, DRAWINGS (OPSD) AND SPECIFICATIONS (OPSS).
- ALL WORKS SHALL BE COMPLETED IN ACCORDANCE WITH THE "OCCUPATIONAL HEALTH AND SAFETY ACT".
- THE LOCATION OF ALL EXISTING UNDERGROUND AND ABOVEGROUND UTILITIES AND STRUCTURES ARE NOT NECESSARILY SHOWN ON THE CONTRACT DRAWINGS, AND WHERE SHOWN, THE ACCURACY OF THE LOCATION OF SUCH UTILITIES AND STRUCTURES IS NOT GUARANTEED. BEFORE STARTING THE WORK, THE CONTRACTOR SHALL REQUEST LOCATES TO UNDERSTAND THE LOCATION OF ALL SUCH UTILITIES AND STRUCTURES AND SHALL ASSUME ALL LIABILITY IF DAMAGED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO NOTIFY ALL UTILITY COMPANIES PRIOR TO COMMENCING WORK & CO-ORDINATE CONSTRUCTION ACCORDINGLY.
- THE CONTRACTOR SHALL BE FINANCIALLY RESPONSIBLE FOR THE RESTORATION AND THE REPAIR OF EXISTING UTILITIES IF DISTURBED DURING CONSTRUCTION.
- ALL DIMENSIONS, ELEVATIONS AND ALIGNMENT SHALL BE CHECKED AND VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ANY CONSTRUCTION. THE CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE CONSULTANT IMMEDIATELY. ALL DIMENSIONS ARE SHOWN IN S.I. UNITS UNLESS OTHERWISE NOTED.
- ALL BACKFILL FOR SEWERS, CULVERTS, WATERMAINS AND UTILINES ON THE ROAD ALLOWANCE MUST BE MECHANICALLY COMPACTED TO 100% STANDARD PROCTOR DENSITY, UNLESS OTHERWISE RECOMMENDED BY GEOTECHNICAL ENGINEER.
- EROSION AND SEDIMENT CONTROL MEASURES TO BE IN PLACE PRIOR TO START OF ANY CONSTRUCTION, AND MUST BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENT CONTROL PLAN.
- ROAD OCCUPANCY PERMIT IS NOT VALID UNTIL THE SIGNED COPY IS RECEIVED BY THE MTO AND A COPY OF PERMIT AND UTILITY LOCATES MUST BE ON SITE AT ALL TIMES.
- UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER, THE MINIMUM PAVEMENT STRUCTURE (COMPACTED DEPTH) SHALL BE PLACED PER THE TYPICAL SECTIONS.
- TACK COATING TO BE PLACED ALONG EXISTING ASPHALT PER OPSS 308 (JULY 2023).
- GRANULAR 'A' AND GRANULAR 'B' TYPE III SHALL CONFORM TO OPSS.PROV 1010 (APR 2013) AND SP 110S06 (JUN 2020).
- APPROVED CONSTRUCTION PLANS MUST BE KEPT WITH PERMIT ON SITE.
- A COPY OF THE PERMIT OR WRITTEN NOTIFICATION OF COMPLETION OF WORK MUST BE PROVIDED TO THE MTO FOR FINAL APPROVAL (OCCUPANCY PERMIT ONLY).
- POSITIVE DRAINAGE SHALL BE MAINTAINED DURING ALL CONSTRUCTION OPERATIONS.
- ALL CHANGES AND/OR DEVIATIONS FROM THE SUBMITTED APPROVED PLANS AND/OR LOCATIONS SHALL BE SUBJECT TO RE-APPROVAL BY THE MTO.
- ALL NECESSARY CLEARING AND GRUBBING SHALL BE COMPLETED BY THE CONTRACTOR. REVIEW WITH CONTRACT ADMINISTRATOR AND THE MTO PRIOR TO ANY TREE CUTTING.
- ALL PAVEMENT MARKINGS AND SIGNAGE WORK SHOULD BE DONE IN ACCORDANCE WITH BOOK 5 (REGULATORY SIGNS), BOOK 6 (WARNING SIGNS), BOOK 11 (PAVEMENT, HAZARD AND DELINEATION MARKINGS) AND OPSS.PROV 710 (NOV 2023).
- ALL REQUIRED SIGNS MUST BE ORDERED BY MTO.
- CONTRACTOR TO ENSURE USAGE OF ACTIVE OPSS/SP.

B) RESTORATION DETAILS

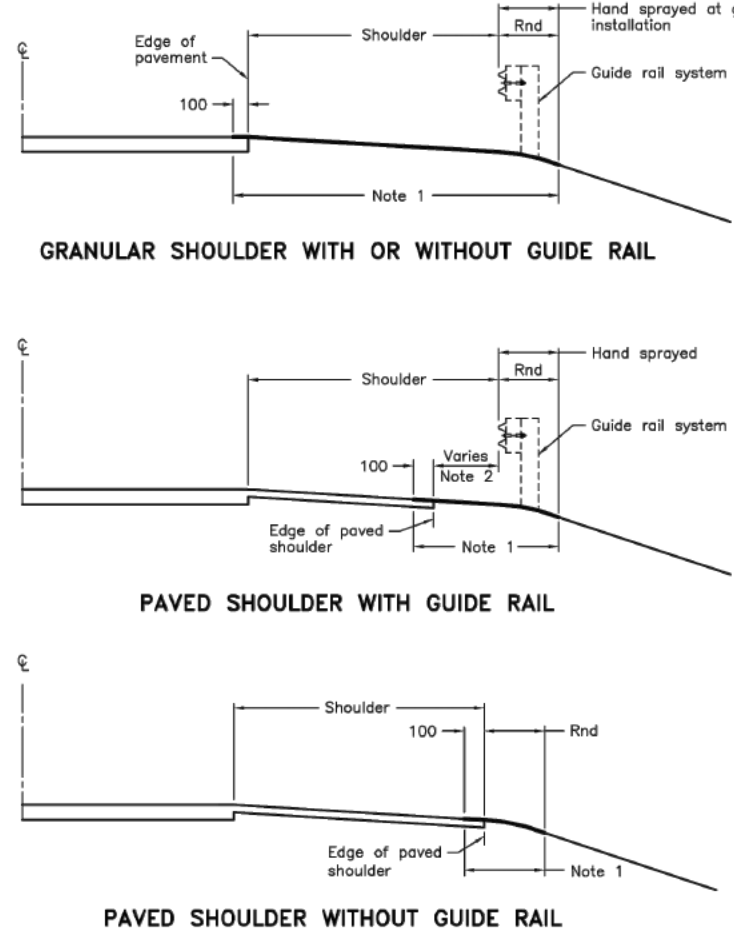
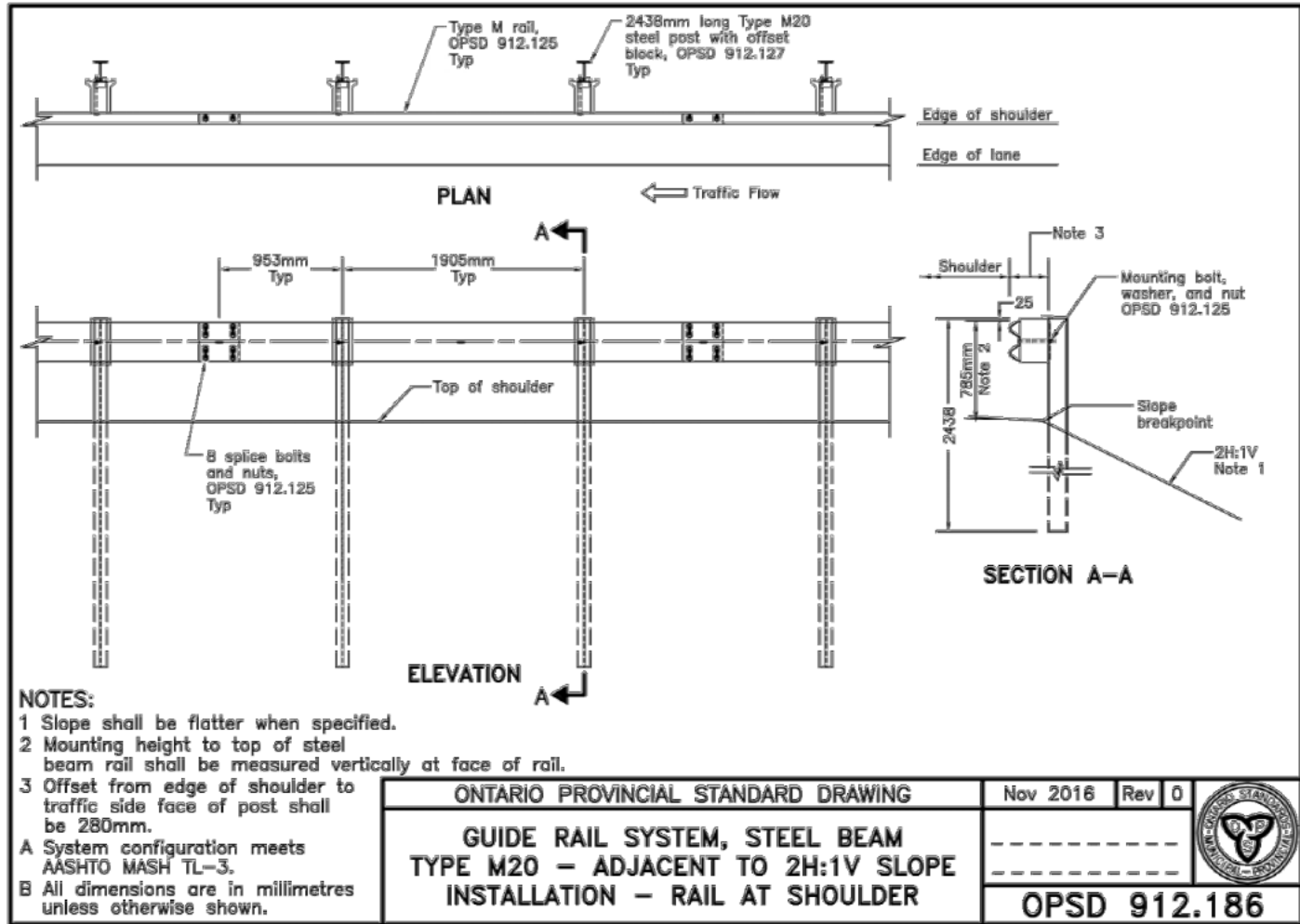
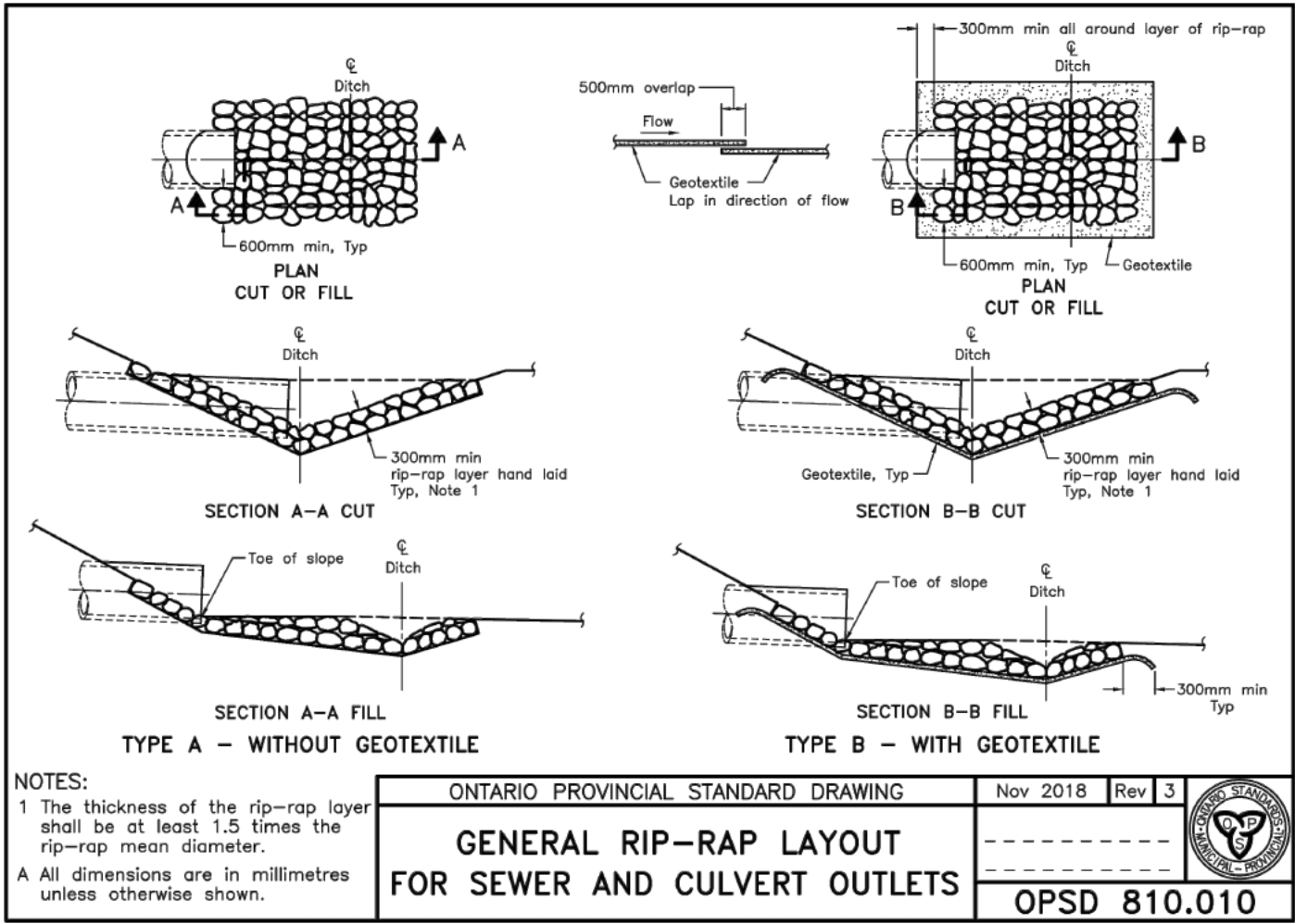
- DISTURBED PORTIONS OF ROAD TO BE REINSTATED TO ORIGINAL CONDITION OR BETTER.
- GRANULAR MATERIALS FOR THE ROAD STRUCTURE TO BE COMPRISED OF 400MM GRANULAR 'B' TYPE III AND 150-350MM GRANULAR 'A', UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER.
- ASPHALT MATERIALS FOR THE ROAD STRUCTURE TO BE COMPRISED OF 75MM SP 19.0, 75MM SP 19.0, AND 50MM SP 12.5 FC1, UNLESS OTHERWISE RECOMMENDED BY THE GEOTECHNICAL ENGINEER.
- REQUIREMENTS FOR PLACEMENT, COMPACTION, AND ACCEPTANCE OF HOT MIX ASPHALT SHALL CONFORM OPSS.PROV 313 (APR 2021), OPSS.PROV 1151 (APR 2021) AND OPSS 1101 (NOV 2020), MTO SPECIAL PROVISIONS SP 103F56. PLEASE REFER AND ADHERE TO SPECIAL PROVISIONS 103F03 AND 111F06 IN THE CONTRACT DOCUMENTS.
- TOP ASPHALT REHABILITATION TO BE DONE TO THE FULL ROAD WIDTH WITHIN LIMITS OF CONSTRUCTION. MILLING OF 50mm OF TOP ASPHALT AND PAVING WITH SP 12.5 FC1 TO APPLY.
- ALL AREAS BEYOND THE RIGHT-OF-WAY (ROW) LIMITS WHICH ARE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO ORIGINAL CONDITION OR BETTER.
- CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY MTO PRIOR TO ANY CONSTRUCTION WITHIN THE MTO ROW.
- CONTRACTOR SHALL COORDINATE WITH PROPERTY OWNERS A MINIMUM OF 24 HOURS IN ADVANCE OF ANY DISRUPTIONS TO DRIVEWAYS AND/OR ACCESS TO PROPERTIES.
- ALL DISTURBED AREAS TO BE REINSTATED TO GRADE WITH 100mm TOPSOIL, MULCH AND SEED (PER OPSS PROV. 803). SEED MIX USED SHALL BE THE SOUTHERN ONTARIO NATIVE SEED MIX. ALSO, SOD SHALL BE PLACED (PER OPSS.PROV. 803) WHERE IT IS CURRENTLY EXISTS DUE TO AESTHETIC REASONS.

C) GEOTECHNICAL RECOMMENDATIONS

- EXCAVATION FOR PAVEMENT WIDENING SHOULD BEGIN AT THE EXISTING EDGE OF PAVEMENT. FULL DEPTH PAVEMENT STRUCTURE WIDENING SHOULD BEGIN AT THE EXISTING SHOULDER ROUNDING AND BE CONSTRUCTED WITH EARTH FILL FROM THE FROST DEPTH TO THE SUBGRADE LEVEL TO ENSURE SIMILAR SUBGRADE CONDITIONS AS THE EXISTING PAVEMENT STRUCTURE. REFER TO TYPICAL SECTIONS.
- PARTIAL DEPTH REMOVAL OF THE EXISTING FULLY PAVED SHOULDERS AND FRASER CRESCENT TAPER IS REQUIRED PRIOR TO CONSTRUCTING THE NEW WIDENING, AS DISPLAYED IN THE REMOVALS PLAN.
- FULL DEPTH REMOVAL OF SIDE STREET ENTRANCES ARE REQUIRED PRIOR TO CONSTRUCTING THE NEW WIDENING, AS DISPLAYED IN THE REMOVALS PLANS.
- MILLING IS REQUIRED TO PREPARE STEP JOINTS IN THE EXISTING ASPHALT ALONG THE WIDENING AREA. SEE TYPICAL SECTIONS FOR JOINT CONSTRUCTION DETAILS.
- GRADING SHALL BE CARRIED OUT PER OPSS.PROV 208 (NOV 2014).
- FOR THE LANE WIDENING, THE NEW PAVEMENT STRUCTURE THICKNESS WILL BE PLACED TO MATCH OR EXCEED THAT OF THE EXISTING TOTAL PAVEMENT STRUCTURE THICKNESS.
- PRIOR TO EMBANKMENT WIDENING, THE SURFICIAL VEGETATION / TOPSOIL SHOULD BE STRIPPED FROM THE EXISTING EDGE OF SHOULDER THROUGH TO THE LIMIT OF GRADING FOR THE WIDENING.
- WHEN ENCOUNTERED, PEAT MUST BE FULLY REMOVED. EARTH BORROW TO ADHERE TO OPSS.PROV 212 (NOV 2013).
- SUBGRADE PREPARATION SHOULD INVOLVE REMOVAL OF ANY EXCESSIVELY WET SOIL, TOPSOIL, FILL, PEAT AND / OR OTHER DELETERIOUS MATERIALS, PROOFROLLING THE EXPOSED SUBGRADE TO MINIMUM 95% STANDARD PROCTOR MAXIMUM DRY DENSITY AND REPLACEMENT WITH SELECT SITE MATERIAL AS REQUIRED TO ACHIEVE THE DESIGN SUBGRADE ELEVATION.
- WHERE GRADES NEED TO BE RAISED, APPROVED MATERIAL SHOULD BE PLACED PER OPSS.PROV 501 (NOV 2014). EARTH BORROW SHALL CONFORM TO OPSS.PROV 212 OR SITE SOIL CAN BE UTILIZED.
- THE PLACEMENT OF EMBANKMENT MATERIAL SHOULD BE CARRIED OUT IN ACCORDANCE WITH OPSS.PROV 206 (NOV 2014).
- EMBANKMENT SLOPES CONSTRUCTED OF 2H:1V OR FLATTER ARE EXPECTED TO BE STABLE. EFFORT SHOULD BE MADE TO USE 3H:1V EARTH SLOPES WHERE POSSIBLE.
- NEW DITCHING AND SHORT EMBANKMENT SLOPES WILL BE CONSTRUCTED OUT OF ON-SITE SOIL, WHICH HAS A VERY SLIGHT ERODIBILITY.
- AS A MINIMUM, THE NEW SLOPES SHOULD BE TOPSOILED, SEEDED, MULCHED AND / OR SODDED AS SOON AFTER GRADING AS POSSIBLE TO PREVENT EROSION.
- EROSION CONTROL BLANKETS SHOULD BE USED TO PROTECT ALL EARTH SLOPES STEEPER THAN 3H:1V PER OPSS.PROV 804 (APR 2021).
- THE EXISTING SHOULDERS WILL BE EXCAVATED FOR CONSTRUCTION OF NEW FULL DEPTH GRANULARS. THE EXCAVATED GRANULARS ARE ACCEPTABLE TO BE REUSED AS EARTH FILL, IF SUITABLE.
- THE EXCAVATED EARTH MATERIAL CONSISTING OF EXISTING FILL, COMPOSED OF SAND / SILTY SAND IS CONSIDERED SUITABLE FOR REUSE AS EARTH FILL.
- ORGANIC SOILS SHOULD BE REUSED ONLY FOR LANDSCAPING OR SLOPE FLATTENING PURPOSES. OTHERWISE, THE DELETERIOUS MATERIAL OR EXCESS EARTH SHOULD BE DISPOSED OUTSIDE OF THE ROW AND MUST BE IN CONFORMANCE WITH ENVIRONMENTAL LEGISLATION, INCLUDING O. Reg. 406/19: ON-SITE AND EXCESS SOIL MANAGEMENT.
- WHERE POSSIBLE, DITCHING SHOULD BE PROVIDED PRIOR TO WIDENING TO ALLOW TRAPPED WATER TO DRAIN.
- ALL DITCHES (DEPTHS) SHOULD BE CONSTRUCTED TO TIE IN WITH THE ADJACENT DRAINAGE PATTERNS. GRANULAR MATERIAL SHOULD BE CARRIED FULL WIDTH TO DAYLIGHT AT THE DITCHLINE PER OPSD 206.010 (NOV 2018).
- RIP - RAP TO BE PLACED AT CULVERT OUTLETS (SIMILAR TO OPSD 810.010 (NOV 2018)), SHARP BENDS IN DITCH ALIGNMENT, ALONG SECTIONS OF STEEP DITCH GRADIENT (IN EXCESS OF 5%), AND OTHER CRITICAL AREAS OF HIGH VOLUME / VELOCITY FLOW.
- BELOW THE RIP - RAP AN OPSS 1860 (APR 2018) CLASS II NON - WOVEN GEOTEXTILE WITH FILTRATION OPENING SIZE (FOS) OF 75 TO 150 MICROMETRE IS REQUIRED.
- PEAT WAS NOTED BENEATH THE FILL IN BOREHOLES 11 (ROAD) AND 12 (SHOULDER) AND WAS 800 AND 300MM IN THICKNESS, RESPECTIVELY, EXTENDING TO 1.5M DEPTH. WHERE ENCOUNTERED, PEAT MUST BE FULLY REMOVED. OPSS.PROV 212 EARTH BORROW OR SITE SOIL CAN THEN BE PLACED AND COMPACTED TO DESIRED SUBGRADE ELEVATION.
- FROST TREATMENT REQUIREMENTS FOR CULVERTS TO BE IN ACCORDANCE WITH OPSD 803.030 AND 803.031.
- REFER TO OPSD 802.010 AND 802.014 FOR EXCAVATION, EMBEDMENT MATERIAL AND BACKFILL REQUIREMENTS. EMBEDMENT MATERIAL AND BACKFILL SHOULD COMPRISE OF GRANULAR A.

D) TEMPORARY CONDITIONS

- ALL TRAFFIC CONTROLS FOR TEMPORARY CONDITIONS TO BE IN ACCORDANCE WITH LATEST VERSION OF ONTARIO TRAFFIC MANUAL (OTM) BOOK 7.
- CONTRACTOR TO ESTABLISH SAFE TRAFFIC CONTROLS IN ACCORDANCE WITH OTM, BOOK 7 PRIOR TO CONSTRUCTION.
- CONTRACTOR TO PREPARE TRAFFIC PROTECTION PLAN IN ACCORDANCE WITH OTM BOOK 7 FOR APPROVAL BY THE REGULATORY AGENCIES AND CONTRACT ADMINISTRATOR PRIOR TO CONSTRUCTION.
- ALL WORKERS RESPONSIBLE FOR INSTALLING OR REMOVING TRAFFIC CONTROL DEVICES OR MEASURES ARE TO BE AWARE OF OHSA AND TRAINED IN APPLICATIONS OF OTM BOOK 7.
- ALL TRAFFIC CONTROL MEASURES ARE THE RESPONSIBILITY OF THE CONTRACTOR ANY ADJUSTMENTS TO THE TRAFFIC MANAGEMENT PLAN SHALL ADHERE TO OTM BOOK 7.
- CONTRACTOR TO COORDINATE TRAFFIC CONTROL WITH ADJACENT WORKZONES / CONTRACTORS, IF APPLICABLE.

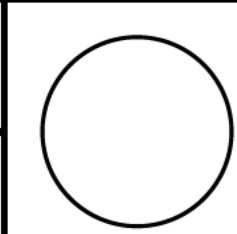


- NOTES:
- Extent of granular sealer.
  - When the width of granular shoulder in front of the guide rail is 300mm or less, granular sealer may be applied by hand spraying for the full width of this application.
- A All dimensions are in millimetres unless otherwise shown.



2024.05.10	R.M.	ISSUED FOR CONSTRUCTION
2024.03.22	R.M.	ISSUED FOR APPROVAL
2024.02.07	R.M.	ISSUED FOR APPROVAL
2024.01.12	R.M.	ISSUED FOR APPROVAL
2023.10.02	R.M.	ISSUED FOR APPROVAL
2023.08.30	R.M.	ISSUED FOR APPROVAL
2023.07.10	R.M.	ISSUED FOR APPROVAL
2023.06.27	R.M.	ISSUED FOR APPROVAL
2023.04.03	R.M.	ISSUED FOR APPROVAL
2022.12.22	R.M.	ISSUED FOR APPROVAL
2022.07.25	R.M.	ISSUED FOR APPROVAL
2022.04.27	R.M.	ISSUED FOR 100% DESIGN REVIEW
2021.12.03	R.M.	ISSUED FOR 90% DESIGN REVIEW
2021.03.17	R.M.	ISSUED FOR 60% DESIGN REVIEW
2020.10.16	R.M.	ISSUED FOR 30% DESIGN REVIEW
DATE	BY	DESCRIPTION
DESIGN I.Z.	CHK R.M.	CODE
DRAWN I.Z.	CHK S.S.	SITE
		DATE
		DWG





LEGEND

----- EXISTING ROW

CL HWY 26							
Number	START STATION	END STATION	START NORTHING EASTING	END NORTHING EASTING	RADIUS (m)	LENGTH (m)	LINE/CHORD DIRECTION
L10	9+331.53	9+863.75	N 4930228.362 E 554076.461	N 4930053.959 E 554579.289		532.21	S70° 52' 16.21"E
C9	9+863.75	10+172.53	N 4930053.959 E 554579.289	N 4930032.216 E 554883.760	588.000	308.78	S85° 54' 55.19"E
L14	10+172.53	10+274.30	N 4930032.216 E 554883.760	N 4930051.565 E 554983.676		101.77	N79° 02' 25.83"E

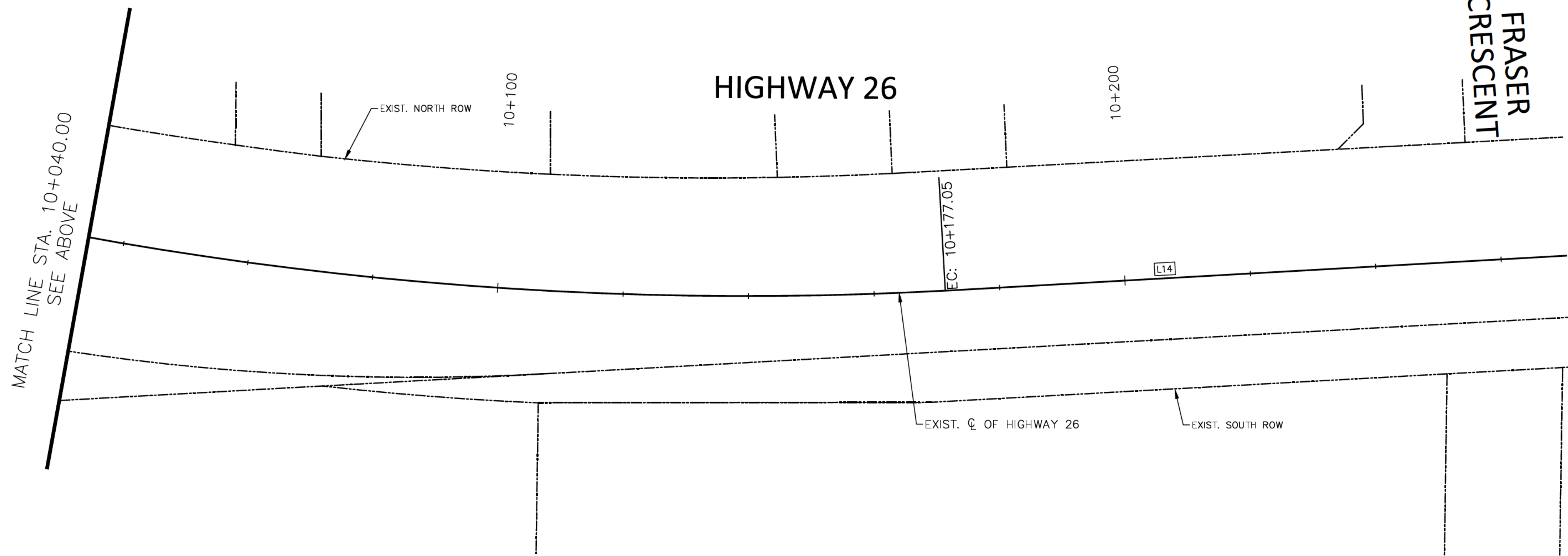
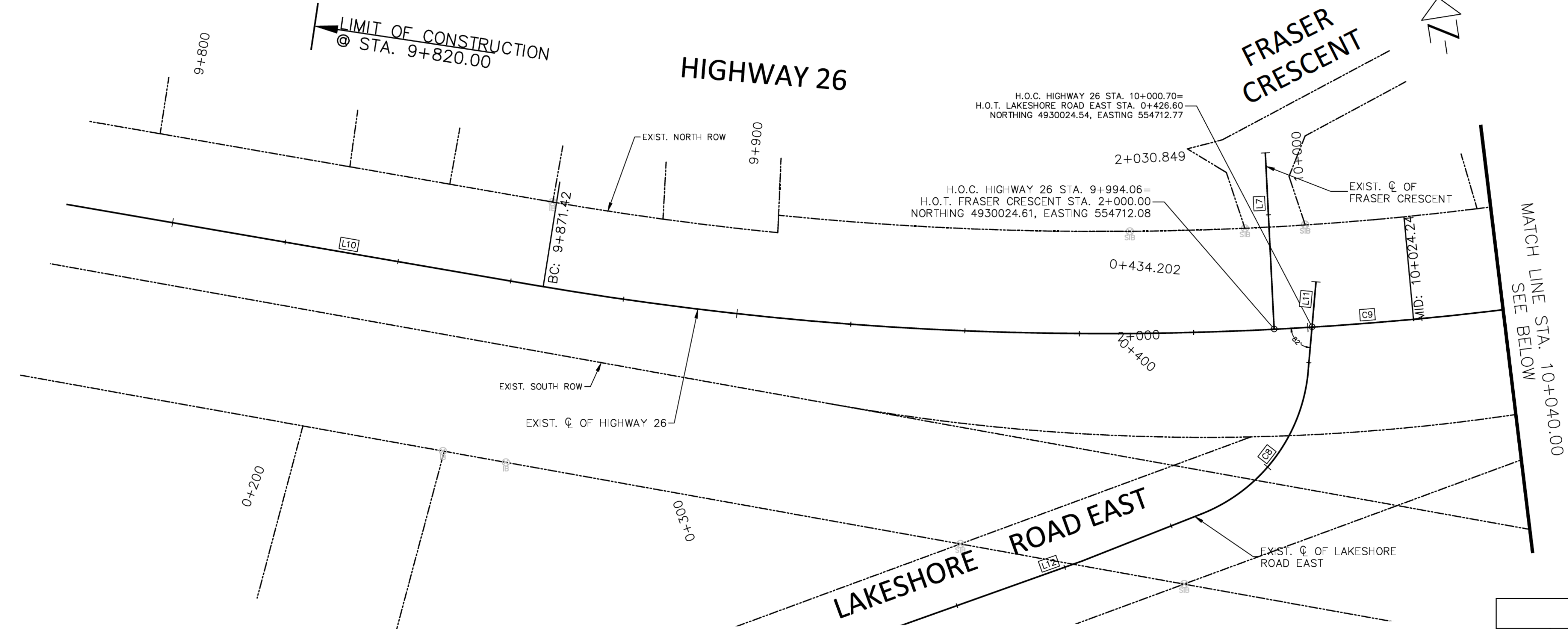
Lakeshore Road East							
Number	START STATION	END STATION	START NORTHING EASTING	END NORTHING EASTING	RADIUS (m)	LENGTH (m)	LINE/CHORD DIRECTION
L12	0+285.89	0+363.26	N 4929976.814 E 554590.335	N 4929990.633 E 554666.459		77.37	N79° 42' 41.18"E
C8	0+385.68	0+418.53	N 4929995.377 E 554688.372	N 4930017.019 E 554710.841	29.677	32.85	N46° 04' 28.98"E
L11	0+418.53	0+434.20	N 4930017.019 E 554710.841	N 4930032.202 E 554714.730		15.67	N14° 22' 00.30"E

CL FRASER CR							
Number	START STATION	END STATION	START NORTHING EASTING	END NORTHING EASTING	RADIUS (m)	LENGTH (m)	LINE/CHORD DIRECTION
L7	2+000.00	2+030.85	N 4930025.241 E 554706.177	N 4930055.895 E 554709.641		30.85	N6° 26' 53.67"E



REVISIONS		DATE	BY	DESCRIPTION
DESIGN	I.Z.	CHK	R.M.	CODE
DRAWN	I.Z.	CHK	S.S.	SITE
				DATE
				DWG
2024.05.10	R.M.	ISSUED FOR CONSTRUCTION		
2024.03.22	R.M.	ISSUED FOR APPROVAL		
2024.02.07	R.M.	ISSUED FOR APPROVAL		
2024.01.12	R.M.	ISSUED FOR APPROVAL		
2023.10.02	R.M.	ISSUED FOR APPROVAL		
2023.08.30	R.M.	ISSUED FOR APPROVAL		
2023.07.10	R.M.	ISSUED FOR APPROVAL		
2023.06.27	R.M.	ISSUED FOR APPROVAL		
2023.04.03	R.M.	ISSUED FOR APPROVAL		
2022.12.22	R.M.	ISSUED FOR APPROVAL		
2022.07.25	R.M.	ISSUED FOR APPROVAL		
2022.04.27	R.M.	ISSUED FOR 100% DESIGN REVIEW		
2021.12.03	R.M.	ISSUED FOR 90% DESIGN REVIEW		
2021.03.17	R.M.	ISSUED FOR 60% DESIGN REVIEW		
2020.10.16	R.M.	ISSUED FOR 30% DESIGN REVIEW		

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**LEGEND**

- EXISTING ROW
- PARTIAL DEPTH PAVEMENT STRUCTURE REMOVAL (PAVED SHOULDER) - FULL DEPTH ASPHALT - 150mm GRANULAR 'A'
- FULL DEPTH PAVEMENT STRUCTURE REMOVAL (SIDEROAD)
- PARTIAL DEPTH ASPHALT REMOVAL (0.30m DEPTH) ASPHALT REHABILITATION
- PARTIAL DEPTH ASPHALT REMOVAL - LONGITUDINAL JOINT
- ASPHALT DRIVEWAY REMOVAL
- GRAVEL DRIVEWAY REMOVAL
- FULL DEPTH REMOVAL FOR SUBBASE EXTENSION (SEE CROSS SECTIONS FOR DETAILS)
- GRAVEL SHOULDER REMOVAL
- TOPSOIL REMOVAL
- ITEM REMOVAL
- STRAW BALE SEDIMENT TRAPS IN DITCH PER OPSD 219.100
- HEAVY-DUTY SILT FENCE PER OPSD 219.130
- NPS 6 STEEL HIGH PRESSURE TRANSMISSION VITAL 3450 kPa MAIN
- NPS 2 PLASTIC INTERMEDIATE PRESSURE DISTRIBUTION 420 kPa MAIN

- NOTES:**
- NPS 6 STEEL HIGH PRESSURE TRANSMISSION VITAL 3450 kPa MAIN IS IDENTIFIED ON ALL APPLICABLE DRAWINGS AND THE CONTRACTOR SHOULD ENSURE APPROPRIATE STANDARDS/PRACTICES ARE ADHERED TO WHEN WORKING IN CLOSE PROXIMITY.
  - NPS 2 PLASTIC INTERMEDIATE PRESSURE DISTRIBUTION 420 kPa MAIN IS IDENTIFIED ON ALL APPLICABLE DRAWINGS AND THE CONTRACTOR SHOULD ENSURE APPROPRIATE STANDARDS/PRACTICES ARE ADHERED TO WHEN WORKING IN CLOSE PROXIMITY.

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REVISIONS	2024.05.10	R.M.	ISSUED FOR CONSTRUCTION
	2024.03.22	R.M.	ISSUED FOR APPROVAL
	2024.02.07	R.M.	ISSUED FOR APPROVAL
	2024.01.12	R.M.	ISSUED FOR APPROVAL
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	2023.08.30	R.M.	ISSUED FOR APPROVAL
	2023.07.10	R.M.	ISSUED FOR APPROVAL
	2023.06.27	R.M.	ISSUED FOR APPROVAL
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	2022.07.25	R.M.	ISSUED FOR APPROVAL
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	2021.12.03	R.M.	ISSUED FOR 90% DESIGN REVIEW
	2021.03.17	R.M.	ISSUED FOR 60% DESIGN REVIEW
	2020.10.16	R.M.	ISSUED FOR 30% DESIGN REVIEW
DATE		BY	DESCRIPTION
DESIGN	I.Z.	CHK R.M.	CODE DATE
DRAWN	I.Z.	CHK S.S.	SITE DWG





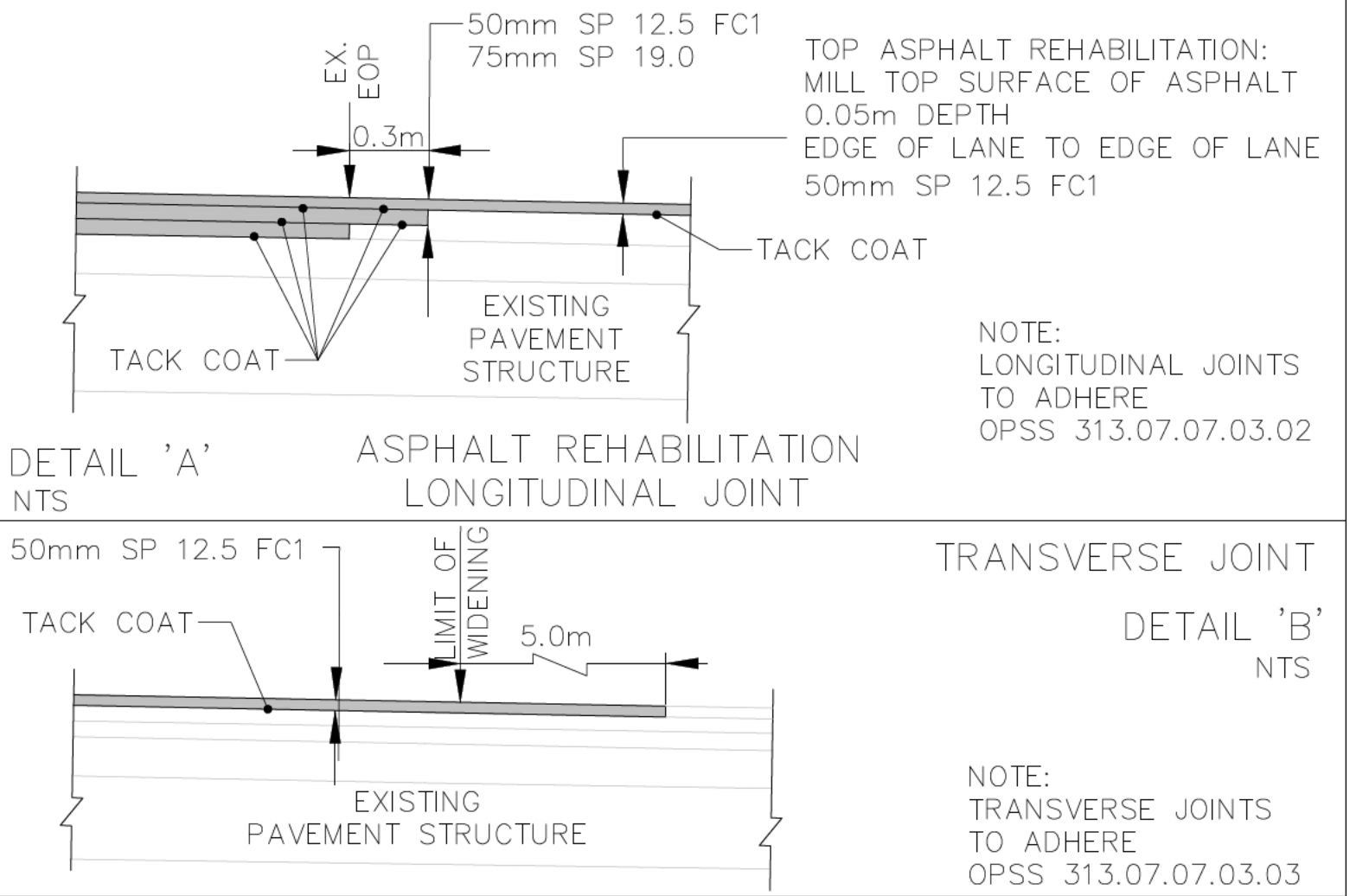






NOTES:

1. PROPOSED SUBGRADE TO MATCH EXISTING TO FACILITATE POSITIVE DRAINAGE. PROVIDE 10:1 TAPER FOR SUBGRADE TRANSITION WHEN NEEDED.
2. PAVEMENT STRUCTURE IS BASED ON GEOTECHNICAL REPORT COMPLETED BY PETO MacCALLUM Ltd. DATED APRIL 2024.
3. REQUIREMENTS FOR PLACEMENT, COMPACTION, AND ACCEPTANCE OF HOT MIX ASPHALT SHALL CONFORM OPSS.PROV 313, OPSS 1101 AND OPSS.PROV 1151, MTO SPECIAL PROVISIONS SP 103F56. PLEASE REFER AND ADHERE TO SPECIAL PROVISIONS 103F03 AND 111F06 IN THE CONTRACT DOCUMENTS.
4. TACK COATING TO BE PLACED ALONG EXISTING ASPHALT PER OPSS 308 (JULY 2023).
5. GRANULAR A AND GRANULAR B TYPE III SHALL CONFORM TO OPSS.PROV 1010 AND SP 110S06.
6. EARTH FILL SHALL CONFORM TO OPSS.PROV 212 EARTH BORROW. WHERE GRADES NEED TO BE RAISED, APPROVED MATERIALS SHOULD BE PLACED PER OPSS.PROV 501, MTO SPECIAL PROVISIONS SP 103F03 AND SP 111F06. THE PLACEMENT OF EMBANKMENT MATERIALS SHALL BE CARRIED OUT IN ACCORDANCE WITH OPSS.PROV 206.
7. EROSION CONTROL BLANKET TO BE PLACED ON SLOPES STEEPER THAN 3H:1V AND ADHERE TO OPSS.PROV 804.
8. SLOPES SHALL BE TOPSOILED, MULCHED AND SEEDED AS SOON AFTER GRADING AS POSSIBLE TO PREVENT EROSION. TOPSOIL AND SHOULD BE A MINIMUM OF 100mm IN DEPTH (PER OPSS.PROV. 803 / 804). ANY SEED MIX USED SHALL BE THE SOUTHERN ONTARIO NATIVE SEED MIX. AREAS WITH MAINTAINED GRASS SHALL BE RESTORED WITH SOD FOR AESTHETIC CONSISTENCY. ALL OTHER AREAS CAN BE RESTORED USING THE APPROVED SEED MIX.
9. RIP-RAP TO BE PLACED AT CULVERT OUTLETS PER OPSP 810.010. BELOW THE RIP-RAP AN OPSS 1860 CLASS II NON-WOVEN GEOTEXTILE WITH FILTRATION OPENING SIZE (FOS) OF 75 TO 150 IS REQUIRED.
10. A HIGH PRESSURE GAS MAIN IS IDENTIFIED ON ALL APPLICABLE DRAWINGS AND THE CONTRACTOR SHOULD ENSURE APPROPRIATE STANDARDS/PRACTICES ARE ADHERED TO WHEN WORKING IN CLOSE PROXIMITY.
11. PRIOR TO INSTALLING GUIDERAIL, THE CONTRACTOR SHALL CONFIRM THAT EXISTING ROGERS LINE IS NOT IN CONFLICT WITH POST INSTALLATION.
12. AROUND STA. 9+940, THE CONTRACTOR SHALL ADJUST LOCALIZED GRADING TO MAINTAIN EXISTING GRADES AROUND EXISTING HP.



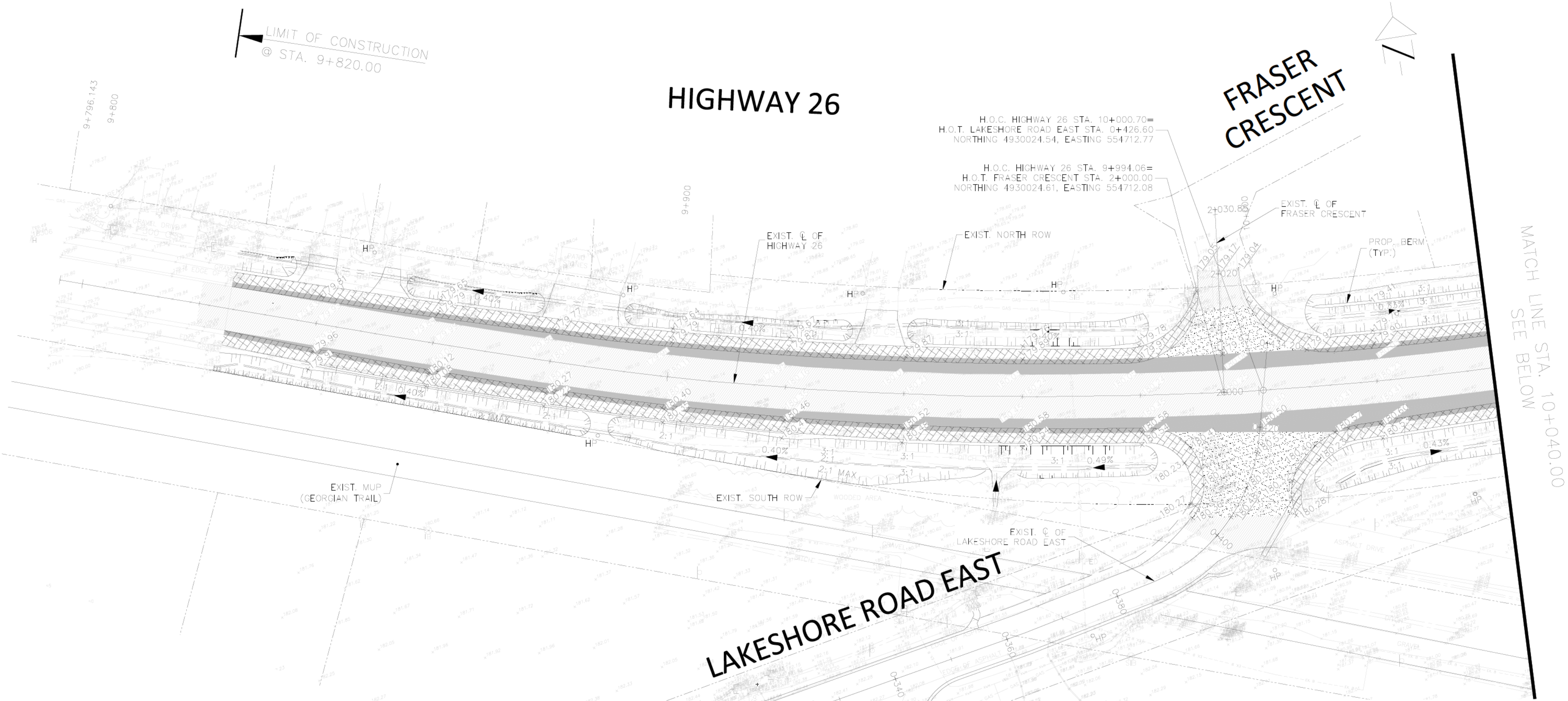
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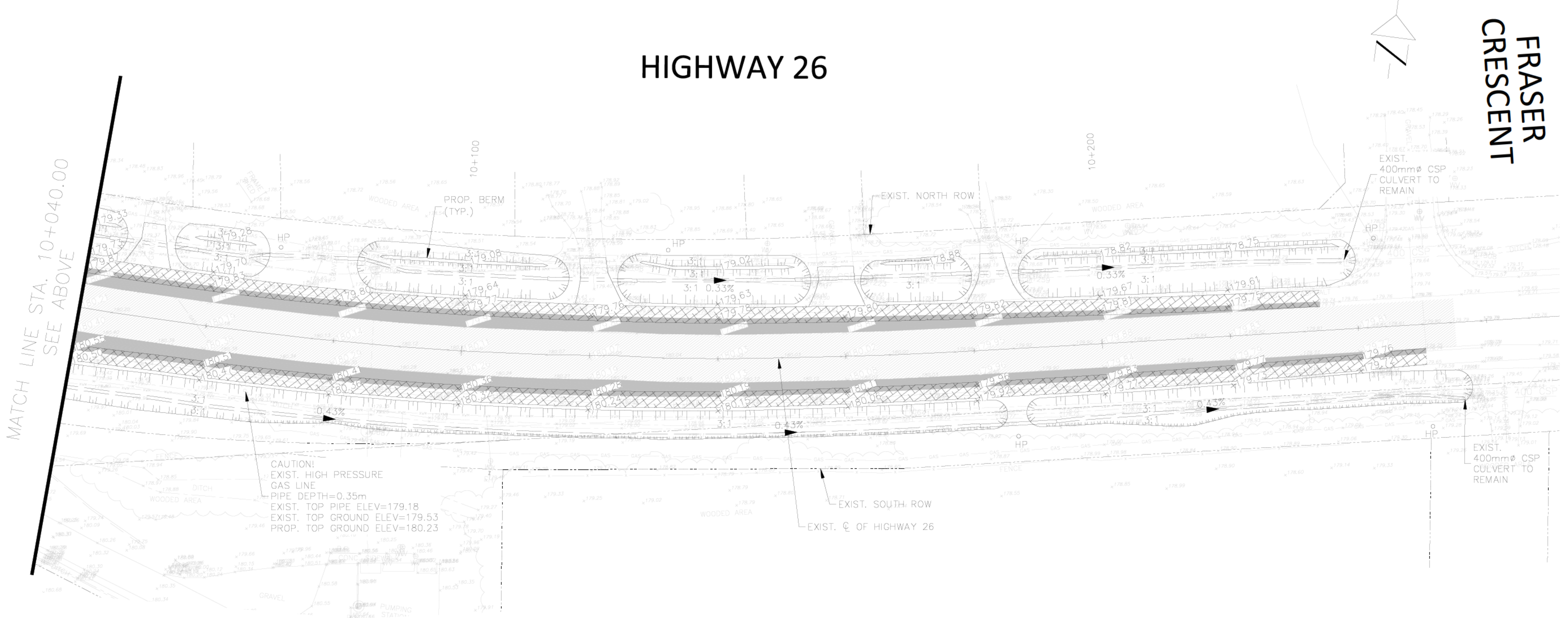
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CRESCENT


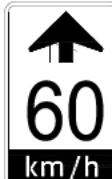







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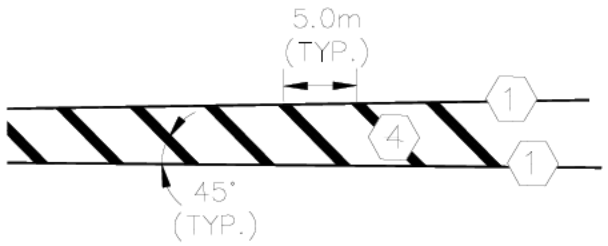
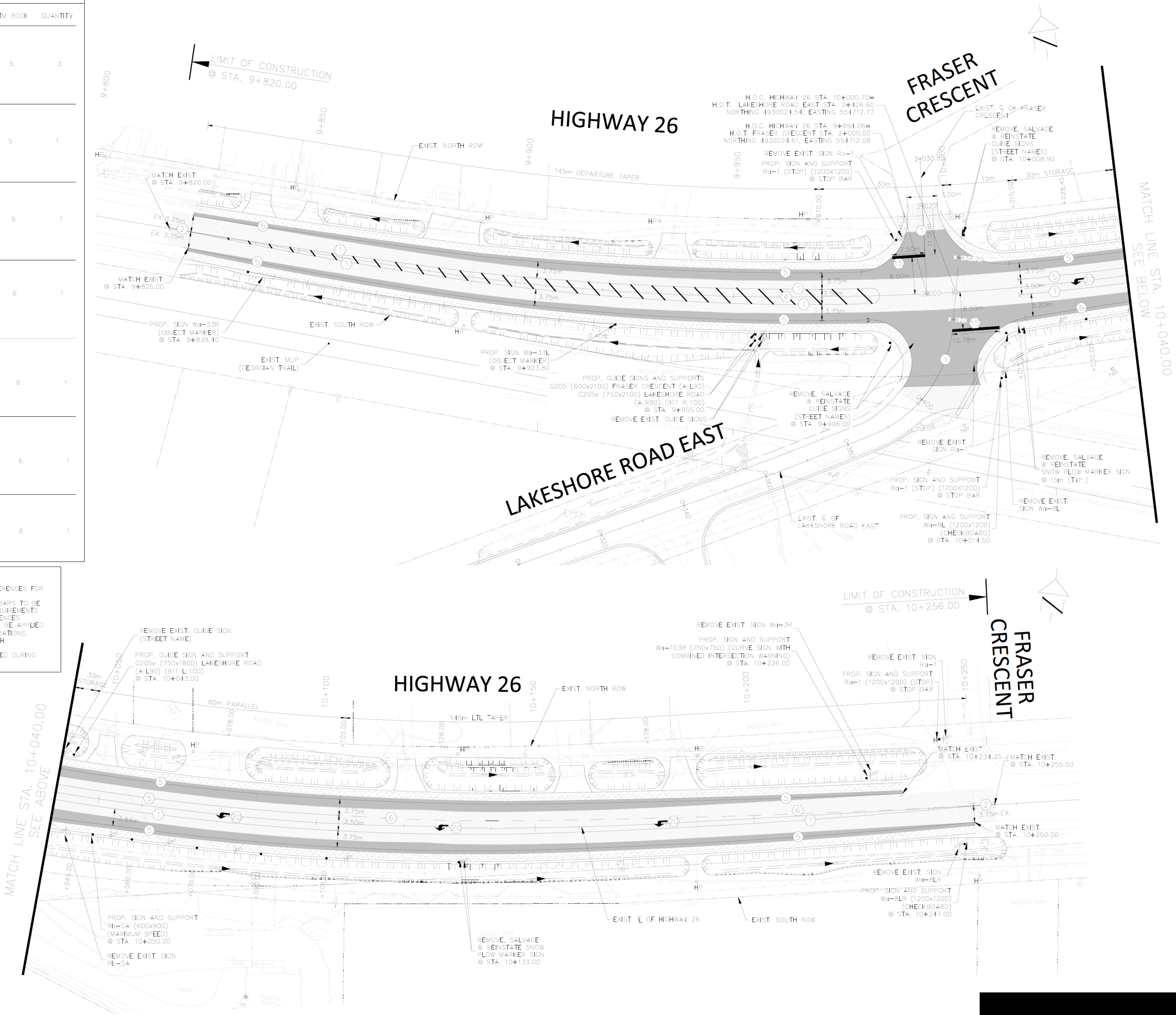
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PROPOSED SIGNAGE LEGEND				
SIGN NAME	SIGN NUMBER	SIZE (mm)	OTM BOOK	QUANTITY
STOP SIGN				
	Ra-1	1200x1200	5	3
MAXIMUM SPEED KM/H AHEAD				
	Rb-5A	600x900	5	1
OBJECT MARKER SIGN (LEFT)				
	Wd-33L	300x900	6	1
OBJECT MARKER SIGN (RIGHT)				
	Wd-33R	300x900	6	1
CURVE SIGN (RIGHT)				
	Wa-103R (MODIFIED)	750x750	6	1
CHECKBOARD ONE DIRECTION				
	Wa-BL	1200x1200	6	1
CHECKBOARD				
	Wa-BLR	1200x1200	6	1

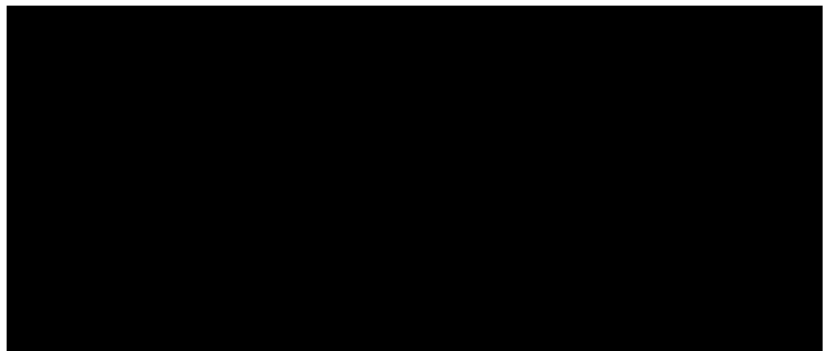
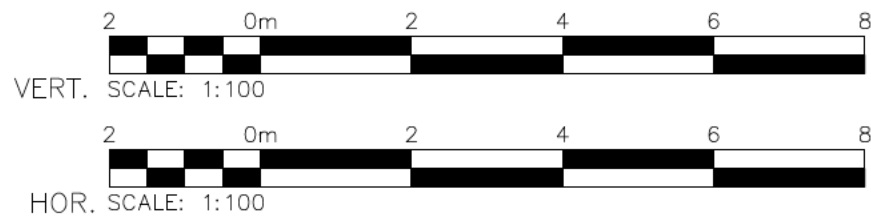
- NOTES:
- SIGNAGE TO FOLLOW WEST REGION DESIGN PREFERENCES FOR SIGNAGE
  - ALL PAVEMENT MARKINGS, SYMBOLS AND STOP BARS TO BE PLACED IN ACCORDANCE WITH OTM BOOK 11 REQUIREMENTS AND WEST REGION PAVEMENT MARKINGS PREFERENCES
  - ALL PAVEMENT MARKINGS AND SYMBOLS ARE TO BE APPLIED PER OPSS 710 AND MANUFACTURE'S RECOMMENDATIONS.
  - ROAD SIGNS TO BE PLACED IN ACCORDANCE WITH OTM BOOKS 5, 6, AND 8.
  - ROADSIDE IDENTIFICATION SIGNS TO BE MAINTAINED DURING CONSTRUCTION.





NOTES:

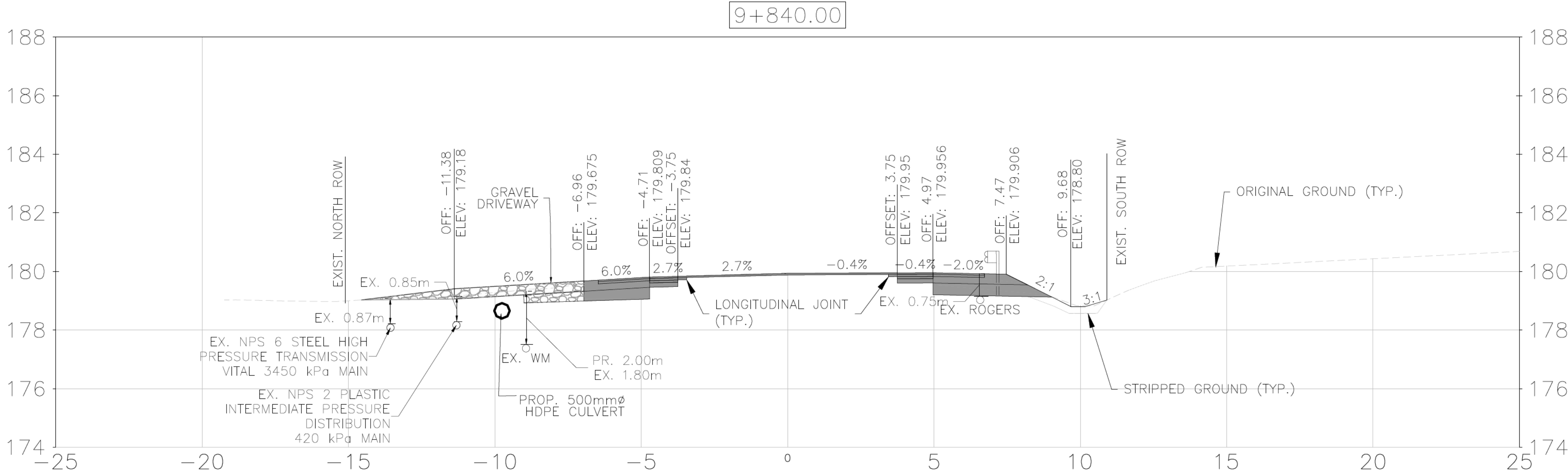
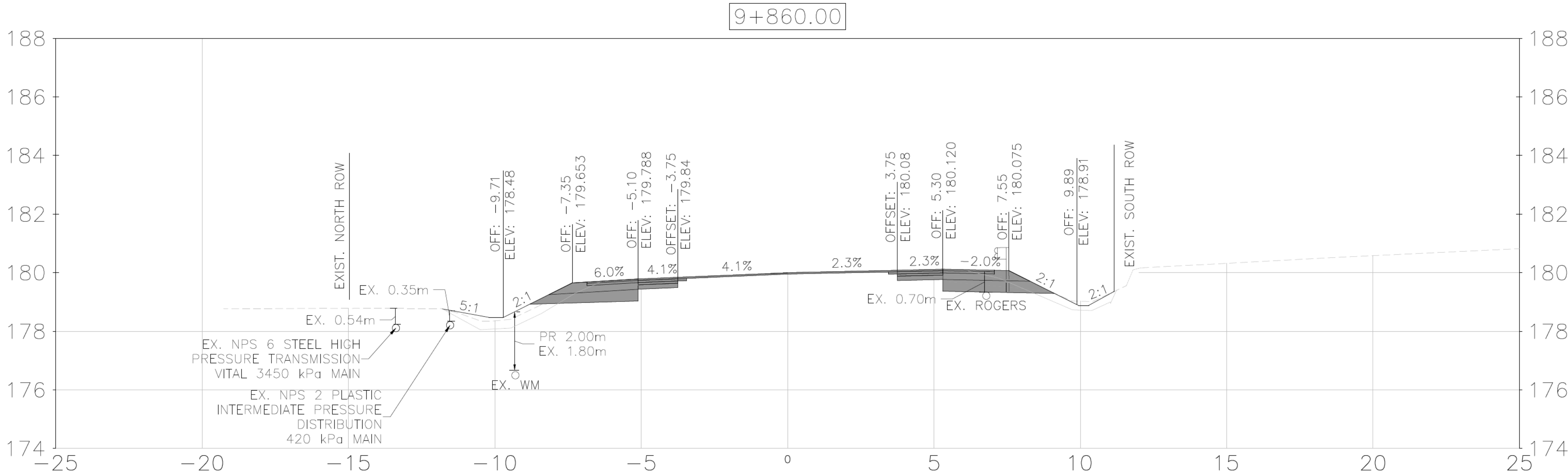
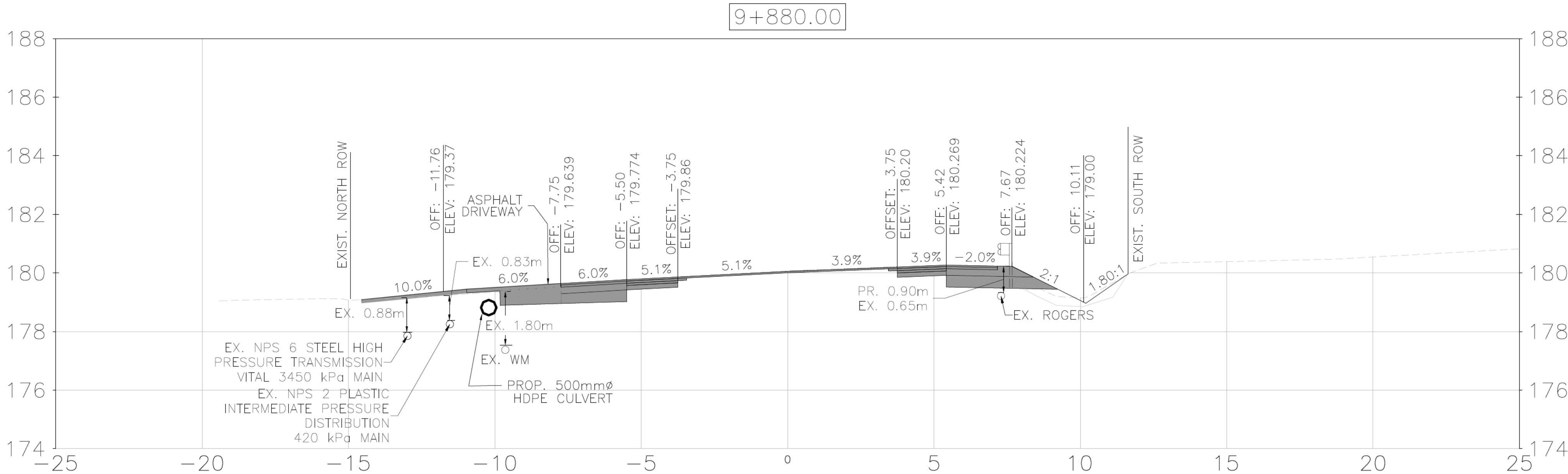
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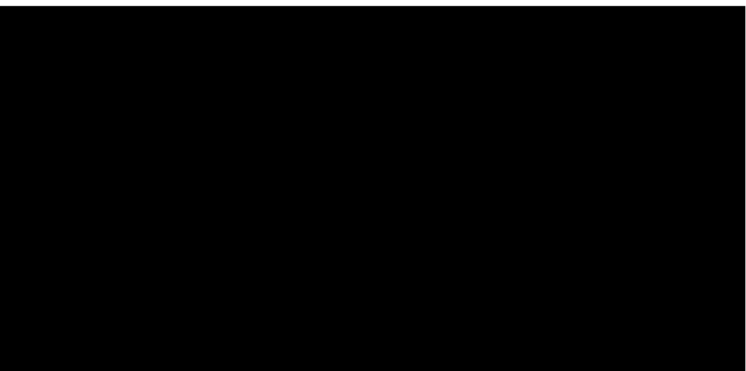
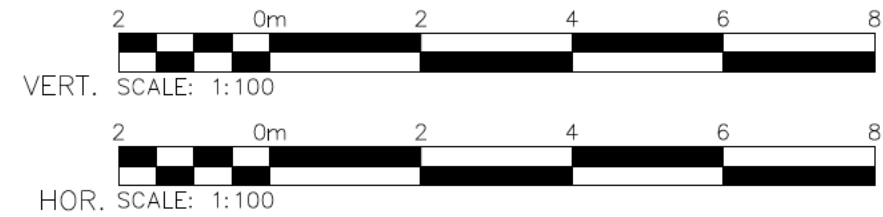
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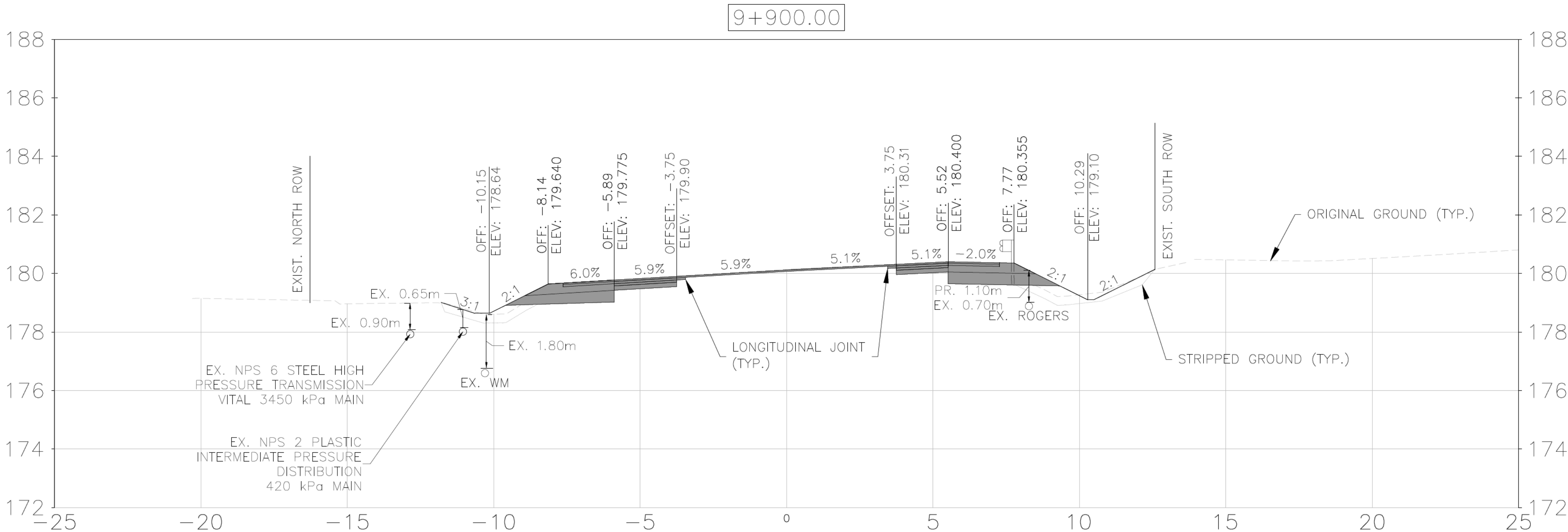
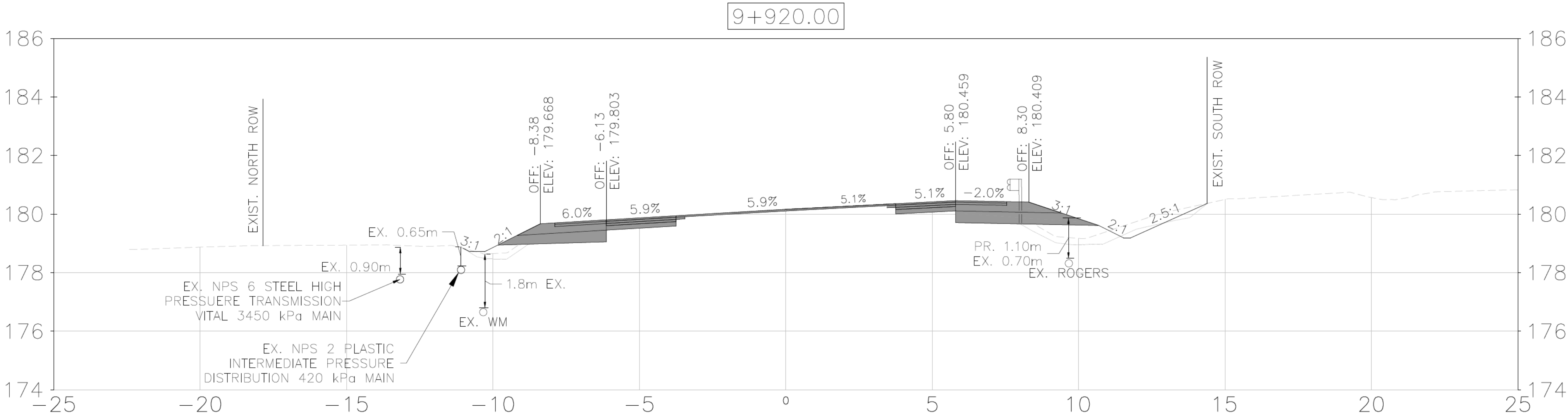
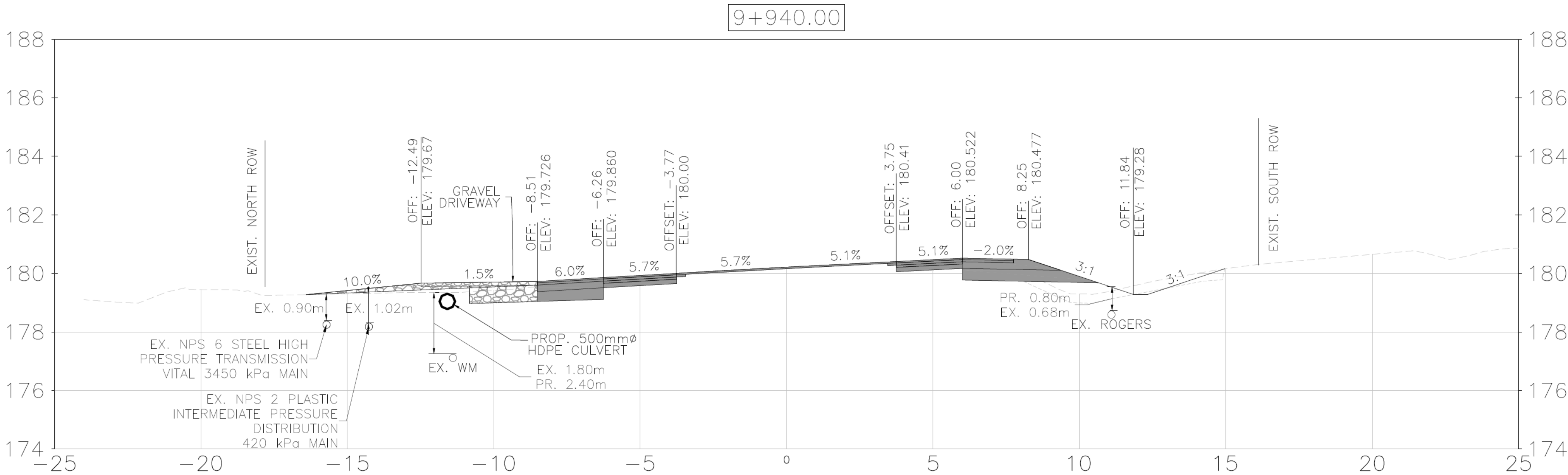
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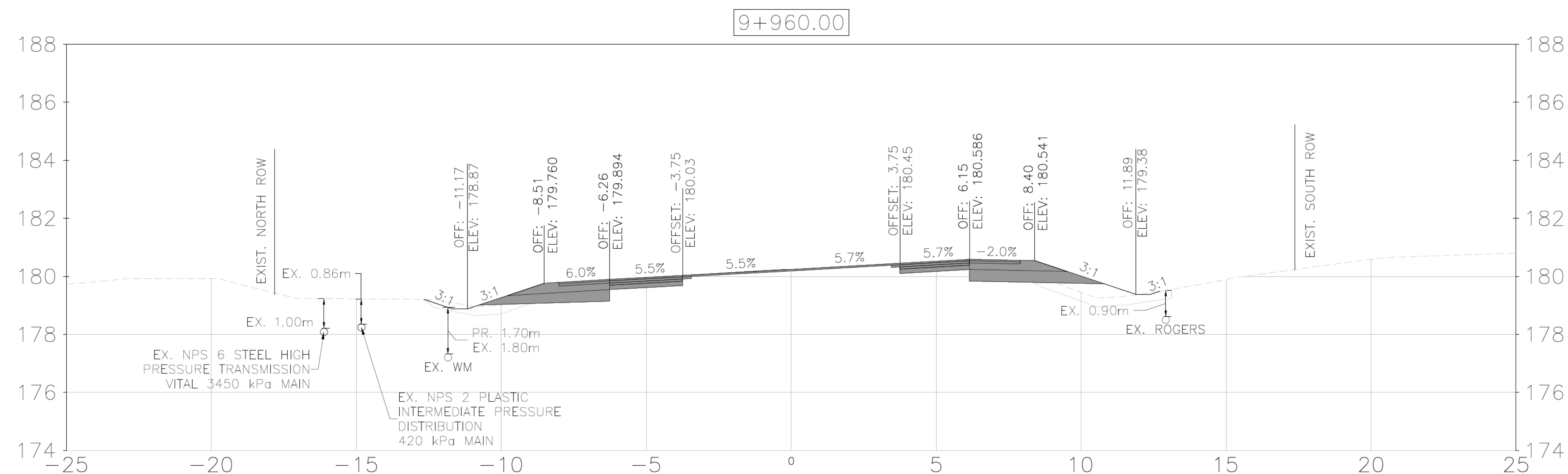
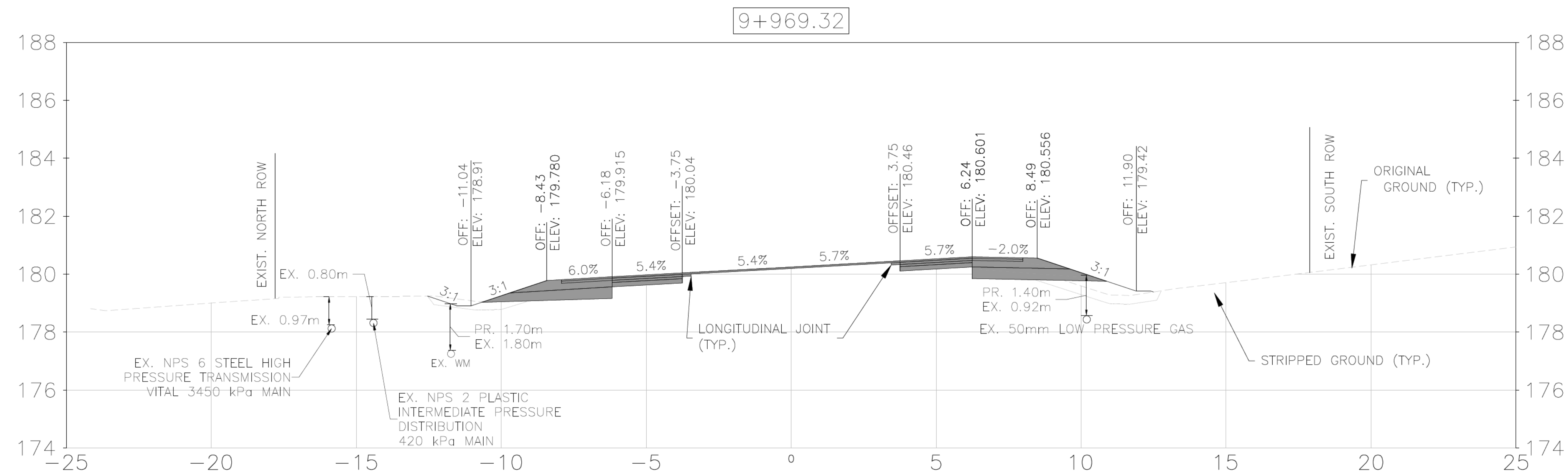
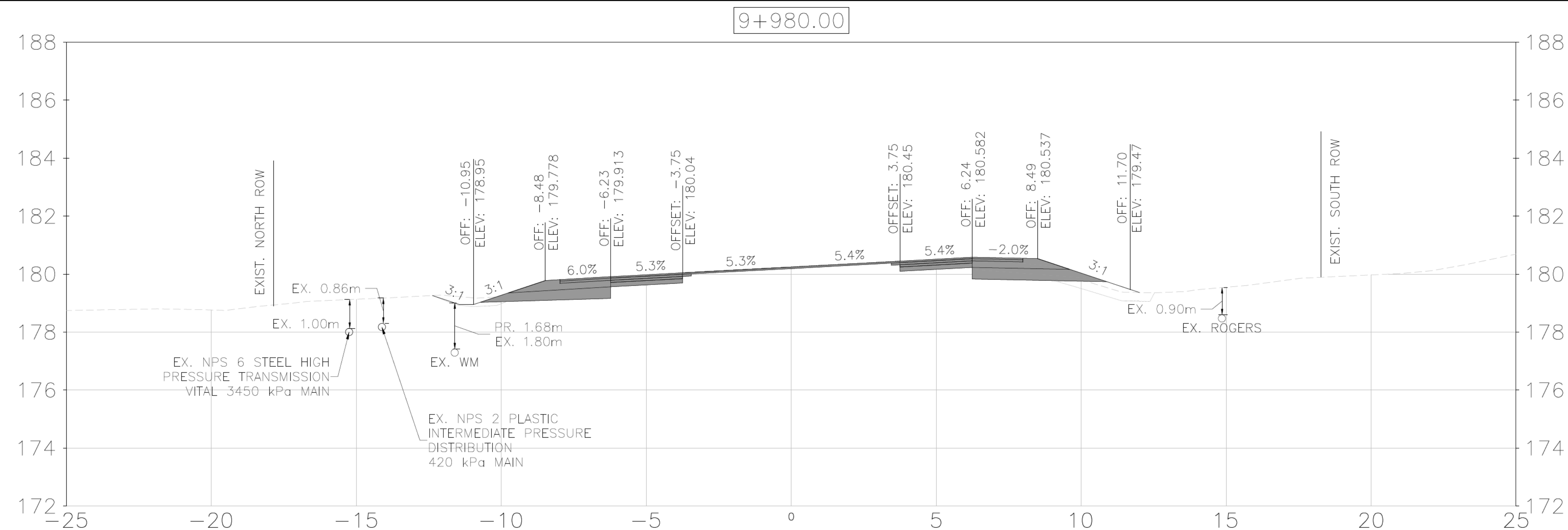
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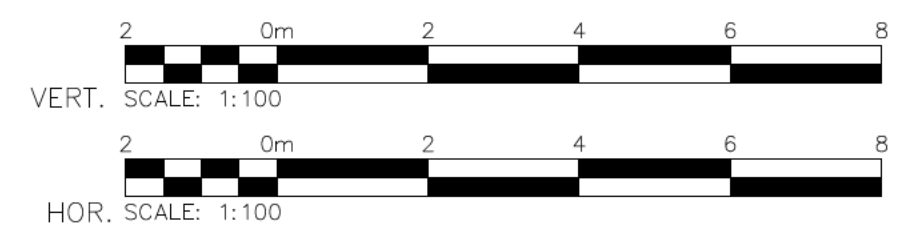
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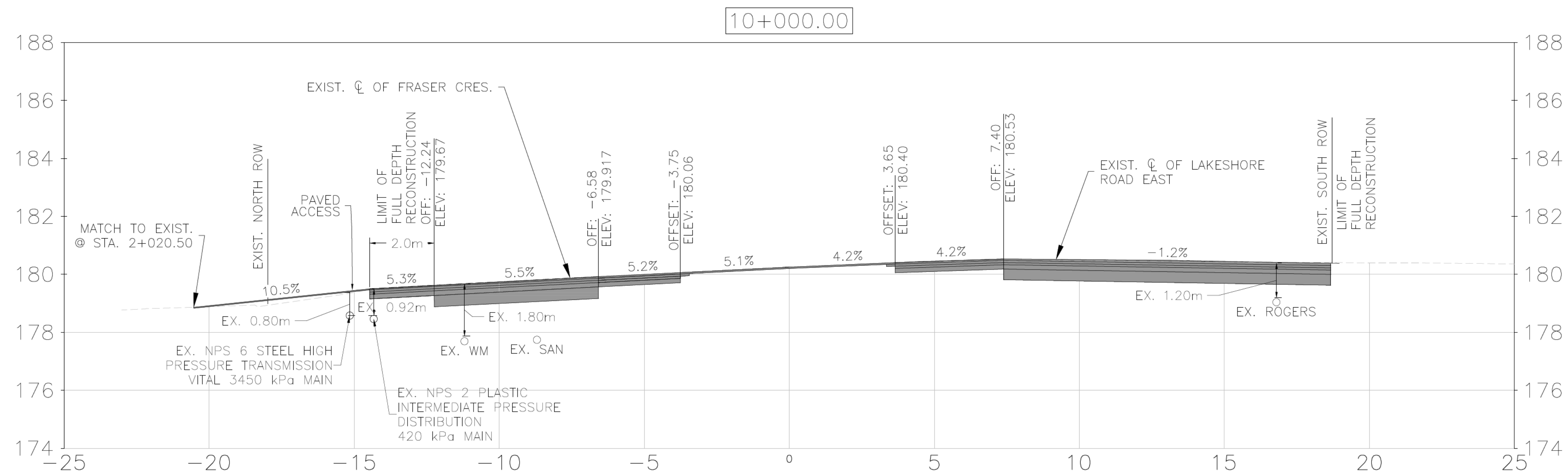
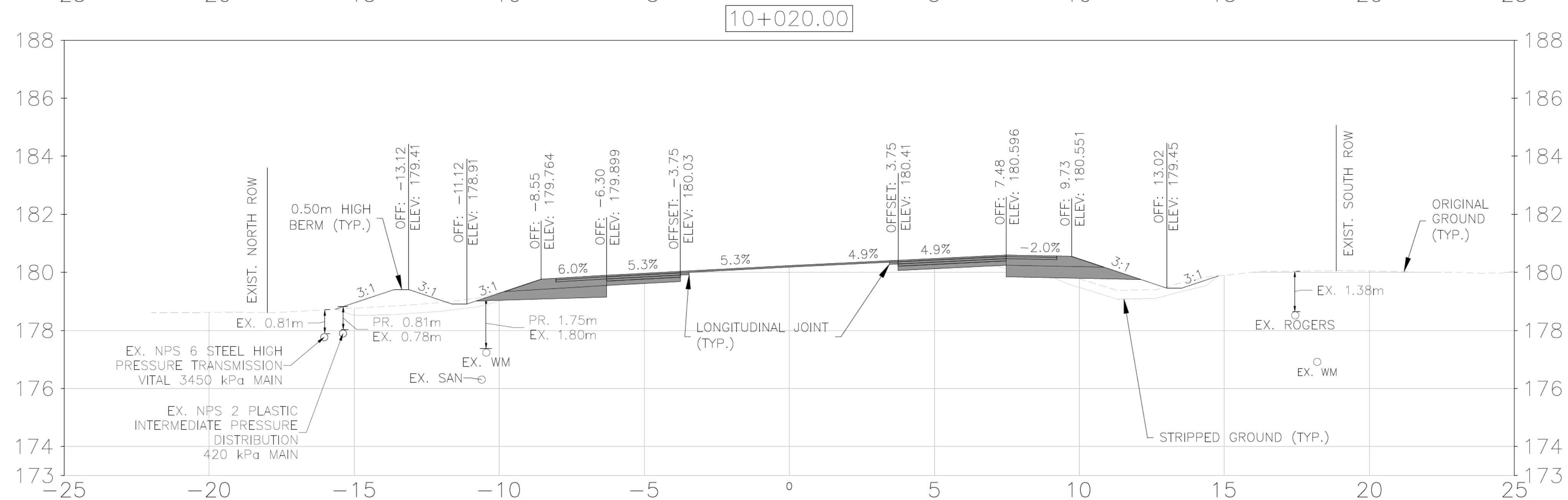
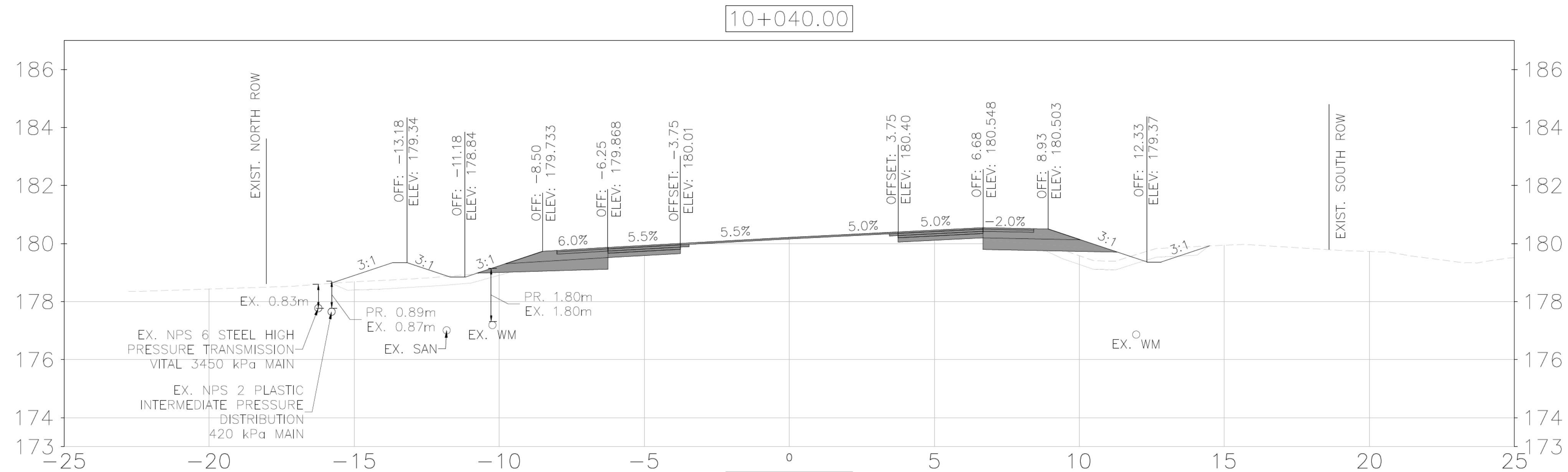


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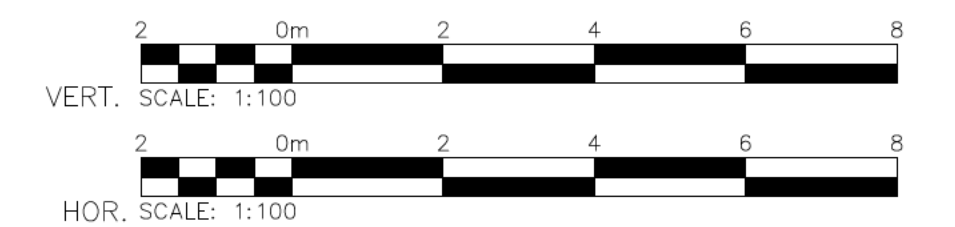


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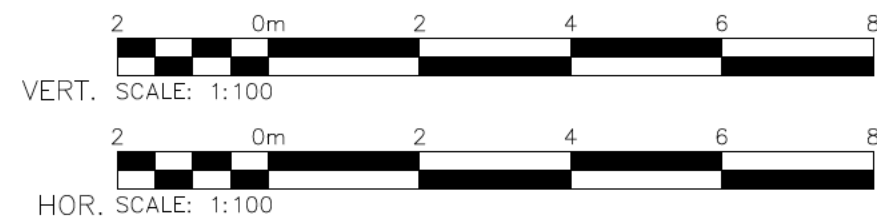


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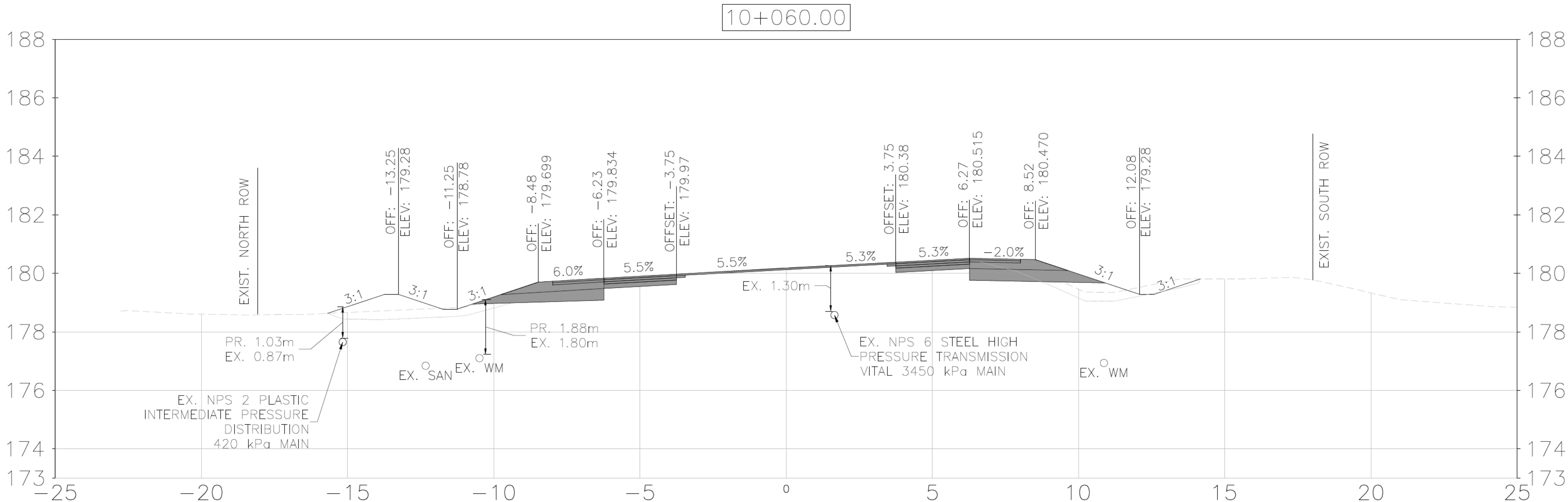
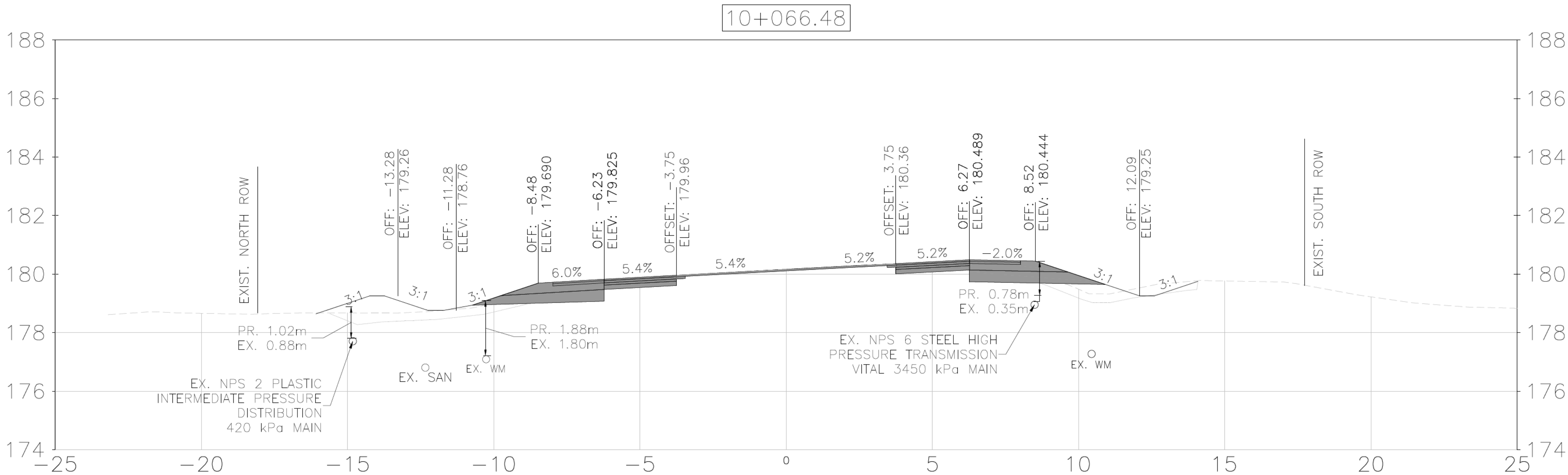
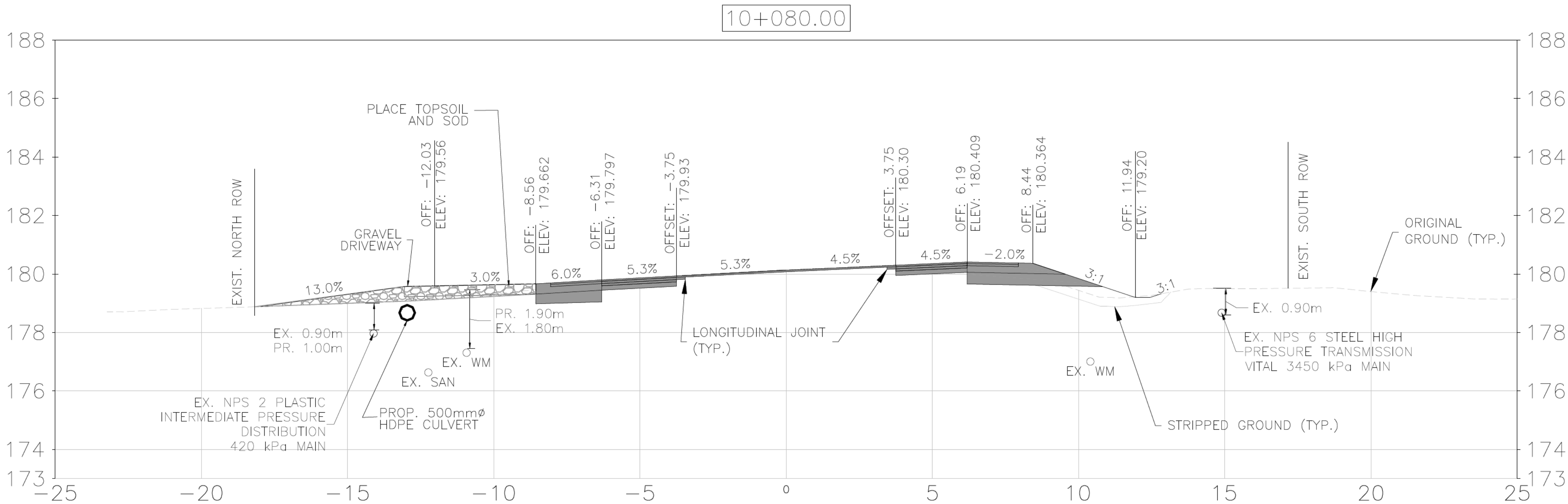
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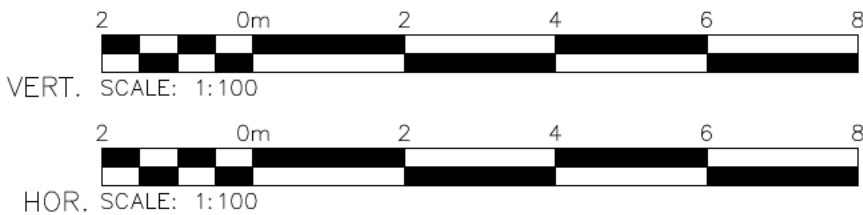
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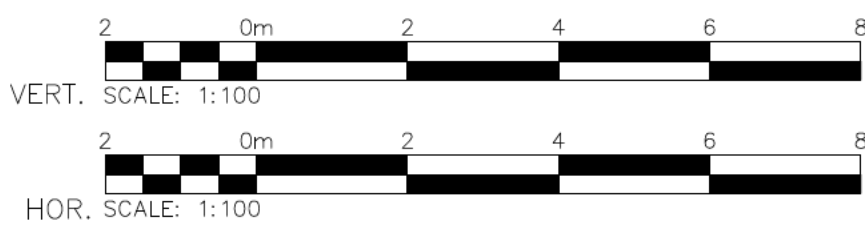
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	2024.01.12	R.M.	ISSUED FOR APPROVAL		
	2023.10.02	R.M.	ISSUED FOR APPROVAL		
	2023.08.30	R.M.	ISSUED FOR APPROVAL		
	2023.07.10	R.M.	ISSUED FOR APPROVAL		
	2023.06.27	R.M.	ISSUED FOR APPROVAL		
	2023.04.03	R.M.	ISSUED FOR APPROVAL		
	2022.12.22	R.M.	ISSUED FOR APPROVAL		
	2022.07.25	R.M.	ISSUED FOR APPROVAL		
	2022.04.27	R.M.	ISSUED FOR 100% DESIGN REVIEW		
	2021.12.03	R.M.	ISSUED FOR 90% DESIGN REVIEW		
	2021.03.17	R.M.	ISSUED FOR 60% DESIGN REVIEW		
	2020.10.16	R.M.	ISSUED FOR 30% DESIGN REVIEW		
DATE		BY	DESCRIPTION		
DESIGN	I.Z.	CHK	R.M.	CODE	DATE
DRAWN	I.Z.	CHK	S.S.	SITE	DWG



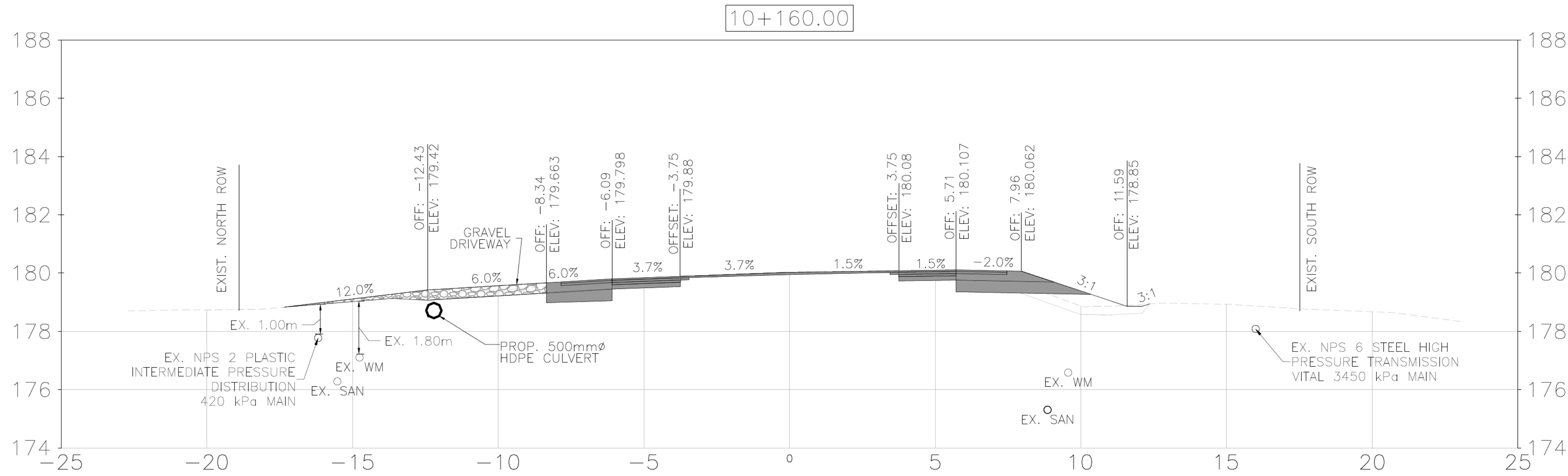
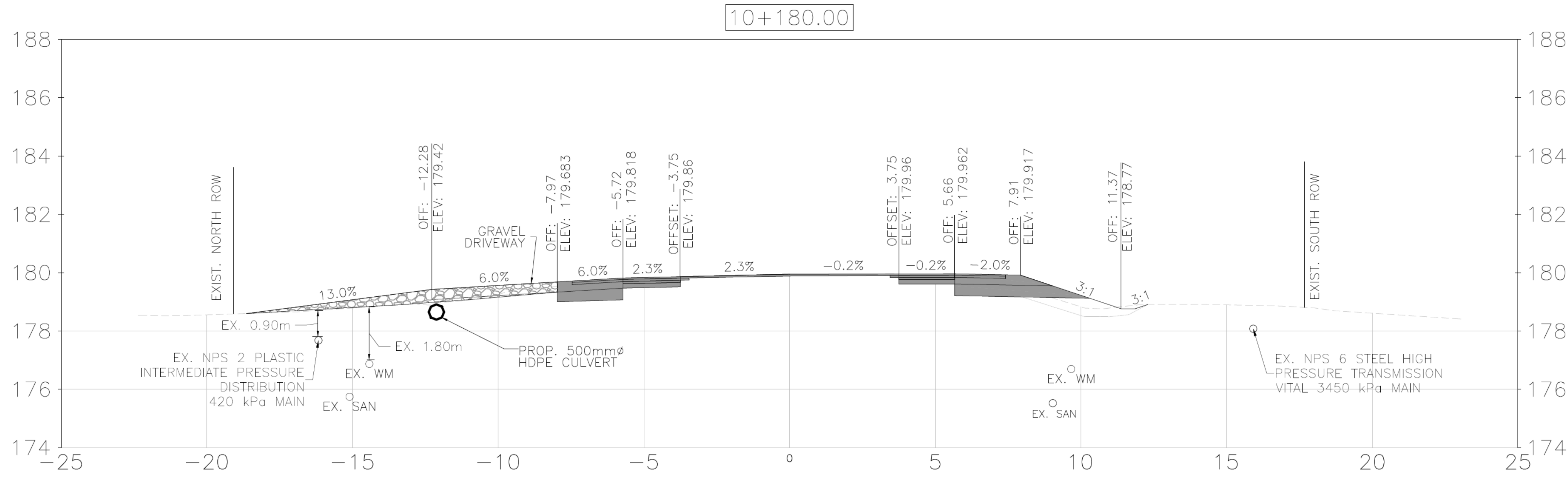
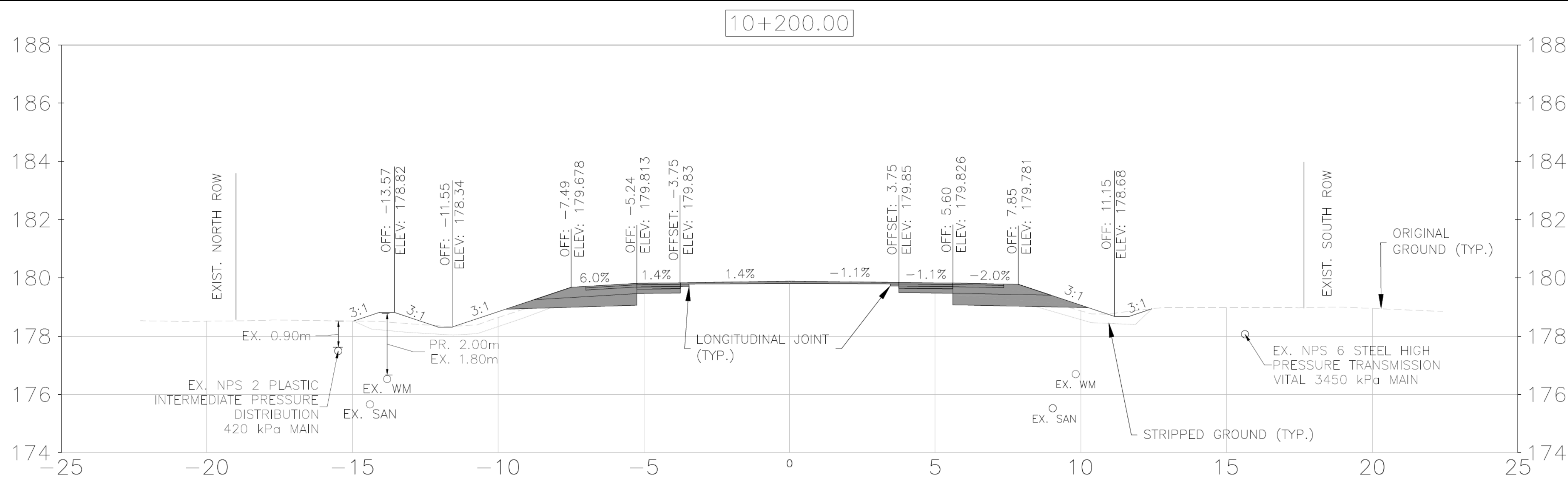
- NOTES:
- NPS 6 STEEL HIGH PRESSURE TRANSMISSION VITAL 3450 kPa MAIN IS IDENTIFIED ON ALL APPLICABLE DRAWINGS AND THE CONTRACTOR SHOULD ENSURE APPROPRIATE STANDARDS/PRACTICES ARE ADHERED TO WHEN WORKING IN CLOSE PROXIMITY.
  - NPS 2 PLASTIC INTERMEDIATE PRESSURE DISTRIBUTION 420 kPa MAIN IS IDENTIFIED ON ALL APPLICABLE DRAWINGS AND THE CONTRACTOR SHOULD ENSURE APPROPRIATE STANDARDS/PRACTICES ARE ADHERED TO WHEN WORKING IN CLOSE PROXIMITY.
  - WHERE UTILITY ELEVATIONS ARE CALLED OUT AT LOCATIONS THAT ARE NOT VERIFIED BY THE TEST PITS, THE DATA HAS BEEN INTERPOLATED FROM THE TWO ADJACENT TEST PITS (REFER TO NEW CONSTRUCTION DRAWINGS).
  - DEPTH OF UTILITIES WERE TAKEN FROM THE TEST PIT REPORT DATED DECEMBER 13th, 2022 PREPARED BY R&B LOCATES.
  - THE LOCATION OF EXISTING UTILITIES ARE APPROXIMATE ONLY.
  - DEPTH OF EXISTING WATERMAIN AND SANITARY WAS NOT VERIFIED BY TEST PITS AND IS ASSUMED 1.80m AND 2.00m MINIMUM, RESPECTIVELY, FROM EXISTING SURFACE.
  - THE CONTRACTOR TO VERIFY THE LOCATION AND ELEVATIONS PRIOR TO CONSTRUCTION.



PROJECT No.: 218-2659

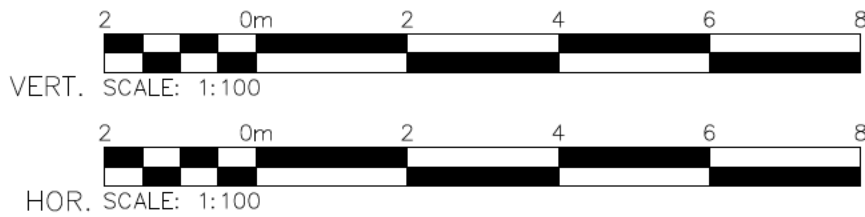
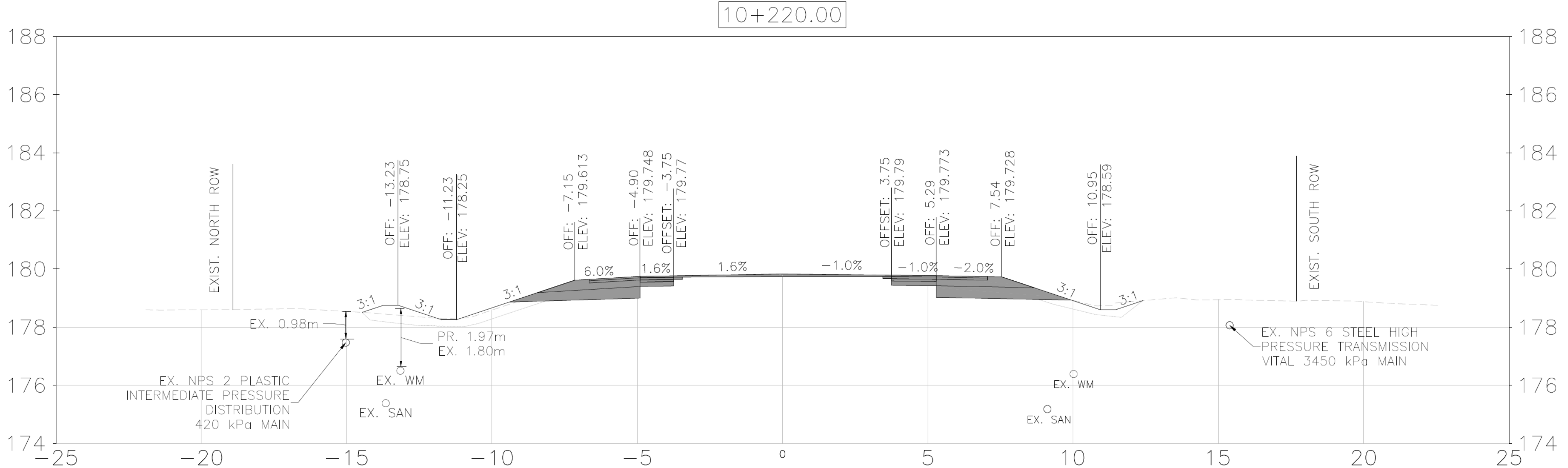
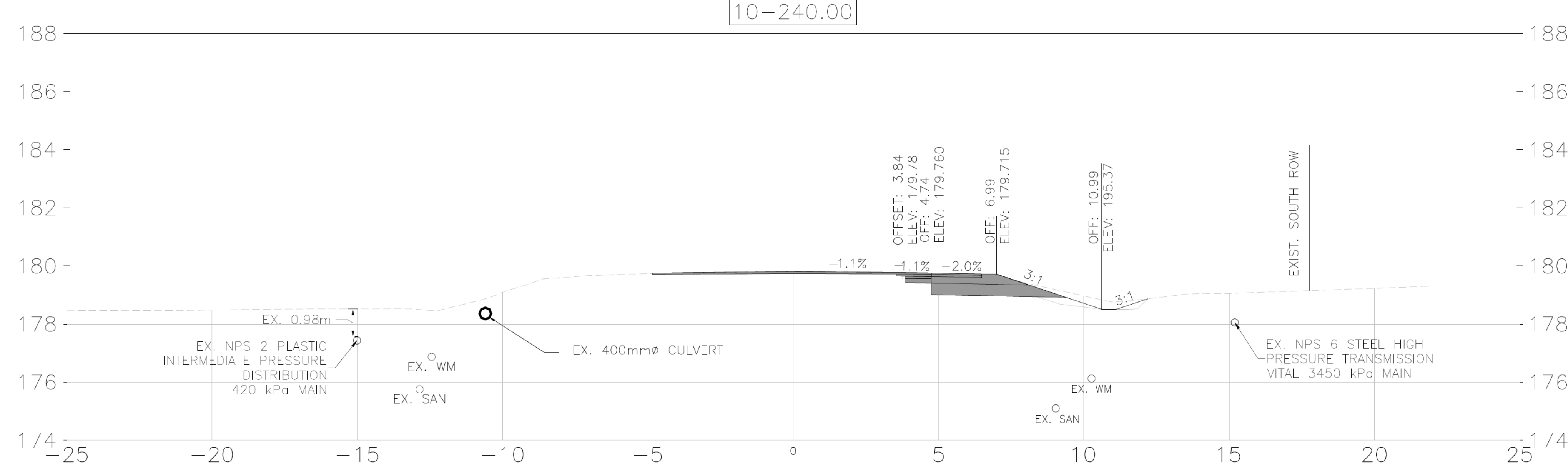


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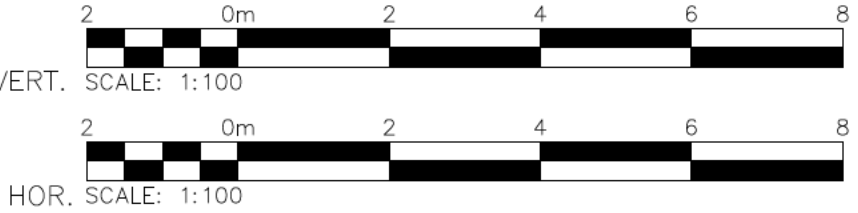
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