



# Staff Report

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## Planning & Building Services – Planning Division

**Report To:** COW - Operations, Planning and Building Services  
**Meeting Date:** June 30, 2026  
**Report Number:** PBS.26.048  
**Title:** Recommendation Report – Community Design Guidelines  
**Prepared by:** Diksha Marwaha, Senior Planner

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### A. Recommendations

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THAT Council receive Staff Report PBS.26.048, entitled “Recommendation Report – Community Design Guidelines”;

AND THAT Council endorse and authorize the use of the Community Design Guidelines attached as Attachment 1 to this report as a Town implementation tool to inform the review of development applications.

### B. Overview

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This report recommends Council’s endorsement of the Community Design Guidelines, which are a guidance tool for new developments. The Design Guidelines are not intended to be used as policies or “regulations” and are applied in a flexible manner for use in staff’s review of development applications.

The Town’s Community Design Guidelines are intended to guide implementation of the Official Plan’s design policies and inform review of development applications.

The Guidelines are not a stand-alone approval instrument, but they assist in assessing whether the proposal achieves the design objectives of the Official Plan and applicable regulations and standards.

Recommended revisions and conditions are tied to applicable Official Plan policies, zoning standards, and matters within the scope of site plan control / draft plan conditions.

The new document includes the following key updates:

- Existing commercial and employment area design guidance has been retained and updated to address current best practices related to built form, site design, streetscape design, and compatibility with surrounding uses.

- New design guidance has been introduced for the Downtown Thornbury, Clarksburg, and Highway 26 (Thornbury) areas to reflect their distinct character and development context.
- Detailed engineering standards previously contained within the guidelines have been removed and are now addressed through the Town's Engineering Standards to avoid duplication.
- Sustainable design principles have been integrated throughout the document rather than being addressed in a standalone section and are highlighted with a distinct icon for ease of reference.

## **C. Background**

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### What are Community Design Guidelines?

Community Design Guidelines are an implementation tool that provides guidance for achieving the community design objectives and policies of the Official Plan. The Guidelines provide detailed direction to shape the built form and public realm in a manner that enhances the quality of life for current and future residents.

The Official Plan establishes the Town's vision, objectives, and land use framework, including policies related to community design. The Zoning By-law regulates permitted uses and development standards.

The Community Design Guidelines have an integrated role alongside the Official Plan and Zoning By-law by providing more detailed design direction and expressing the Town's expectations for the design of new development. The Guidelines translate the Official Plan's design policies into actionable guidance and complement the Zoning By-law by supporting well-designed development.

Together, these documents function as complementary tools to support the vision and objectives of the Official Plan by providing direction for growth and development and promoting good design.

### Purpose of the Project

The first edition of the Town-wide Community Design Guidelines was completed in 2012. Since then, the Town has updated key policy documents, including the Official Plan (2016), the Comprehensive Zoning By-law (2018), and most recently, the Draft Official Plan through the 5-Year Review process.

In light of these updates, the updated Community Design Guidelines are intended to align the Town's design guidance with the Draft Official Plan and the Comprehensive Zoning By-law. The updated Guidelines build on the direction established in the 2012 Guidelines and reflect current policy direction and regulatory standards.

### Implementation - How to use the Design Guidelines

The Design Guidelines are intended to be used as a guidance tool during the planning and design process and throughout the development review process. They support interpretation of the Official Plan and other applicable standards and by-laws. The Design Guidelines do not replace or override the Town's Official Plan, Zoning By-law, Engineering Standards, or any other applicable policies or by-laws.

In applying the Guidelines, consideration should be given to the Town's broader policy framework, including applicable plans, by-laws, standards, and initiatives.

The Community Design Guidelines are a complementary document intended to support the implementation of the Town's Official Plan and related policy framework. They provide detailed design direction to assist in achieving the Town's design objectives but do not establish new regulatory requirements, standards, or policies.

Implementation of the Community Design Guidelines will occur primarily through:

- their use by staff, applicants, and consultants during pre-consultation and the development review process;
- their use as a guidance tool in applying the Town's Official Plan's goals and strategic objectives.

For Town staff, the Community Design Guidelines provide a framework to review and evaluate development applications, helping to ensure that the intended vision for development across the Town's communities is achieved. Staff may require submission of a Community Design Checklist and/or supporting materials to confirm that the applicable Design Guidelines have been considered and incorporated into development proposals.

For applicants and developers, the Guidelines set out the Town's expectations in advance for development and provide direction on how proposals can be designed to meet those expectations.

Overall, the Community Design Guidelines promote consistency while allowing flexibility to respond to different site conditions and project types. They support the creation of a balanced built form that responds to site context and conditions.

#### Public Engagement and Review Process of Community Design Guidelines

The Town retained SGL Planning and Design Inc. to update the existing 2012 Community Design Guidelines and to prepare a Community Design Checklist to support the preparation and review of development applications.

The development of the Community Design Guidelines has been guided by an inclusive and collaborative engagement process, incorporating input from the public, and internal and external stakeholders.

Town staff and the consultant team have worked collaboratively to update and enhance the 2012 Guidelines. On February 3, 2026, an Information Report was presented to the Committee

of the Whole to provide an update on the Community Design Guidelines project. This was followed by a community engagement session on April 14, 2026 and a public meeting on April 21, 2026, where draft Guidelines were presented for feedback from the public and stakeholders. The engagement sessions included a formal presentation by staff, an open question-and-answer period, and informal opportunities for residents to provide feedback directly to the project team. All comments received have been reviewed and addressed, and where appropriate, incorporated into the Guidelines. The comment matrix is attached as Attachment 2 to this report.

### Current Updated Guidelines

The Guidelines provide a clear framework for creating high-quality, sustainable development across the Town. The Community Design Guidelines are organized into following two key components:

**Volume 1-Design Guidelines:** Volume 1 contains the detailed design direction for development. It provides guidance related to natural environment, parks and public spaces, neighborhood design, residential built form, downtown and commercial and employment areas. This volume sets out the specific expectations for how development should be designed at the site and building level.

**Volume 2: Community Vision and Structure:** Volume 2 outlines the overall community design vision, guiding principles, and the structure of the Town's communities, including key settlement areas such as Thornbury, Clarksburg, Craigleith, Lora Bay, Camperdown, and the villages and hamlets. This volume provides broader context for understanding local character and sense of place for each community area, and informs how design direction is applied within each community area.

Volume 1 is intended to be the primary reference for the review and preparation of development applications, while Volume 2 provides supporting context to inform how the Guidelines are applied.

### Next Steps

With Council endorsement, staff will begin implementing the guidelines during the review of new development and redevelopment applications to ensure consistent, sustainable, and cohesive design outcomes.

Staff are also preparing concise, user-friendly design summary materials to support applicants at the outset of the development process (See Attachment 3). These may include checklists or instruction sheets tailored to residential, commercial and other non-residential uses, to highlight key design considerations and Town expectations in a clear and accessible format. This additional step is intended to improve early understanding of the Guidelines, encourage better-designed proposals, and support more consistent implementation through the development review process.

## **D. Strategic Priorities**

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### **1. Communication and Engagement**

We will enhance communications and engagement between Town Staff, Town residents and stakeholders

### **2. Organizational Excellence**

We will continually seek out ways to improve the internal organization of Town Staff and the management of Town assets.

### **3. Community**

We will protect and enhance the community feel and the character of the Town, while ensuring the responsible use of resources and restoration of nature.

### **4. Quality of Life**

We will foster a high quality of life for full-time and part-time residents of all ages and stages, while welcoming visitors.

## **E. Environmental Impacts**

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The Guidelines promote environmentally responsible development by encouraging sustainable design approaches. Direction related to natural features has been updated and enhanced to align with Provincial policy and the Town's policies, helping ensure that development respects and integrates natural systems within the community.

## **F. Financial Impacts**

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There are no new financial implications arising from the planned actions in this report. The cost of the Community Design Guidelines Update project is covered within the existing budget.

## **G. In Consultation With**

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Shawn Postma, Manager of Community Planning and Town Staff

## **H. Public Engagement**

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The topic of this Staff Report has been the subject of a Community Engagement Session which took place on April 14, 2026 and a Public Meeting on April 21, 2026. Those who provided comments at the Public Meeting and Community Engagement Session, including anyone who has asked to receive notice regarding this matter, has been provided notice of this Staff Report. Any comments regarding this report should be submitted to Diksha Marwaha, [planning@thebluemountains.ca](mailto:planning@thebluemountains.ca)

## **I. Attached**

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1. Attachment 1: TBM Community Design Guidelines Volume 1 and Volume 2
2. Attachment 2: Comment Matrix
3. Attachment 3: Community Design Guidelines Checklist

Respectfully submitted,

Diksha Marwaha  
Senior Planner

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### Report Approval Details

Document Title:	PBS.26.048 Recommendation Report - Community Design Guidelines.docx
Attachments:	- Attachment 1 - Community Design Guidelines.pdf - Attachment 2-Comment Response Matrix.pdf - Attachment 3-TBM Guidelines Checklist.pdf
Final Approval Date:	Jun 19, 2026

This report and all of its attachments were approved and signed as outlined below:

**Shawn Postma - Jun 19, 2026 - 9:19 AM**

**Tim Murawsky - Jun 19, 2026 - 1:07 PM**

# Town of The Blue Mountains

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## Community Design Guidelines

### Volume 1 - Design Guidelines



June 2026

Organized as two complementary volumes, the Design Guidelines provide detailed direction for development across the Town's Settlement Areas. **Volume 1** includes the Community Design Guidelines, addressing the Town's expectations for public and private realm design. **Volume 2** provides additional context on the Town's community structure and includes area-specific objectives to guide development and design within each community.

## Volume 1: Guidelines

**Purpose:** To be referenced as applicable for design guidelines and expectations.



### **Section 1 - Introduction:**

Introduces the purpose of the Design Guidelines, who they are for, where they apply and how to use the document.



### **Section 2 - Environment, Parks, Public Spaces and Culture:**

Provides guidelines for development around the natural environment, parks and open spaces, trails, community spaces and public buildings, public art, cultural heritage buildings and gateway features.



### **Section 3 - Neighbourhood Design and Residential Built Form:**

Provides guidelines for neighbourhood design, residential built form, infill and intensification.



### **Section 4 - Downtown and Commercial Areas:**

Provides guidelines for streetscapes, site design and built form in downtown and commercial areas.



### **Section 5 - Urban Employment Areas:**

Provides guidelines for site and building design in the Town's Urban Employment Areas.

## Volume 2: Vision and Community Structure

**Purpose:** To confirm development responds appropriately to its vision and context.



### **Section 1 - Community Design Vision and Guiding Principles:**

Presents a community design vision and guiding principles for the Town to ensure high-quality design of the public and private realm across.



### **Section 2 - Community Structure:**

Describes the character of the Town's different Settlement Areas, or communities, and sets out design objectives and direction for future development.

# Volume 1- Guidelines

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# 1 Introduction

## 1.1 Purpose of the Design Guidelines

The Town of The Blue Mountains embraces a wide diversity of established community, rural and tourism amenities. The Town is generously enriched with natural features and scenic attributes including the Niagara Escarpment, Nipissing Ridge and Nottawasaga Bay, making the Town a very attractive place for residential, recreational and resort development. Each unique community in the Town contributes to the small-town charm and civic identity of The Blue Mountains.



Community design is a key component in creating sustainable, vibrant, age-friendly and inviting places that contribute to a strong sense of place within the Town. The Blue Mountains Community Design Guidelines (Design Guidelines) reflect the Town's commitment to high-quality design that enhances existing character, amenities, sustainability, natural features and cultural heritage resources.

The Design Guidelines are intended to assist in the implementation of The Blue Mountains Official Plan through the development review process. The Design Guidelines express design expectations and provide direction to promote good design of the public and private realm throughout The Blue Mountains.

## 1.2 Who are the Design Guidelines For?

The Design Guidelines are intended to be used by a variety of participants in the design and development process, as well as in the design and implementation of public spaces and buildings.

The Design Guidelines will be used by:



**The Blue Mountains Council** to confirm whether an application meets the Town's vision and its unique communities.



**Town Staff and Agencies** as a reference for the review and approval of development applications, as well as the design of public spaces, streetscape improvements and other municipal projects.



**Developers and Consultants** when planning and designing their projects and development plans in alignment with the Town's vision.



**Landowners and Business Owners** when undertaking site and building improvements.



**The Public** to understand how The Blue Mountains will evolve over time.

## 1.3

# Where do the Design Guidelines Apply?

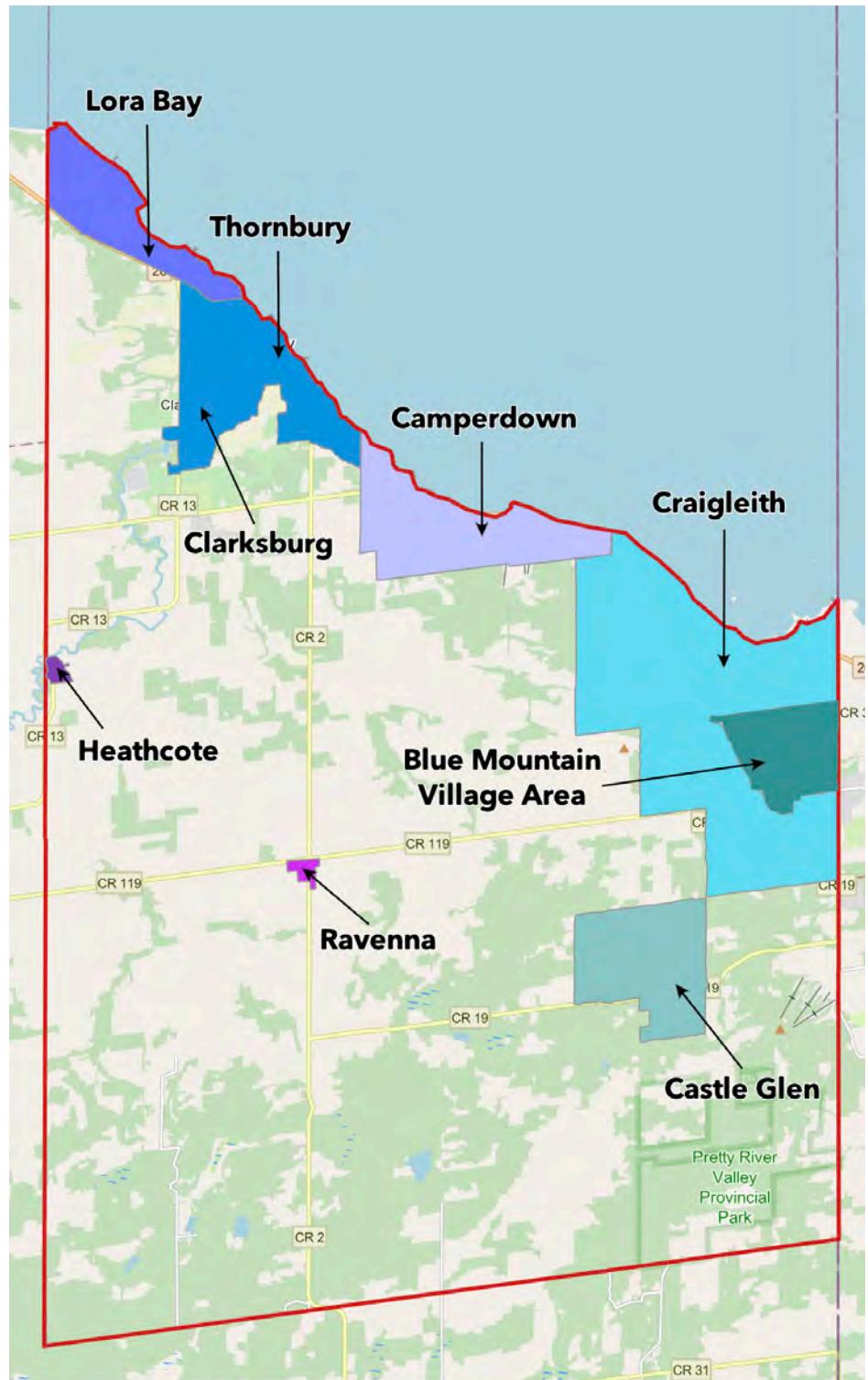
The Design Guidelines apply primarily across the Town's Settlement Areas, where residential, commercial and recreational uses are concentrated, including:

- Thornbury
- Clarksburg
- Craigleith
- Lora Bay
- Camperdown
- Villages and Hamlets

Town-wide direction is provided for residential, commercial, mixed use, employment and open space areas, as well as guidance specific to each community as applicable.

Building on the Town's Official Plan, Volume 2 of the Design Guidelines provides additional context regarding the Town's community structure and includes area-specific objectives to guide development and design within each community.

The Design Guidelines should be read in their entirety and appropriately applied to development within each area of the Town.



The Blue Mountains Settlement Areas

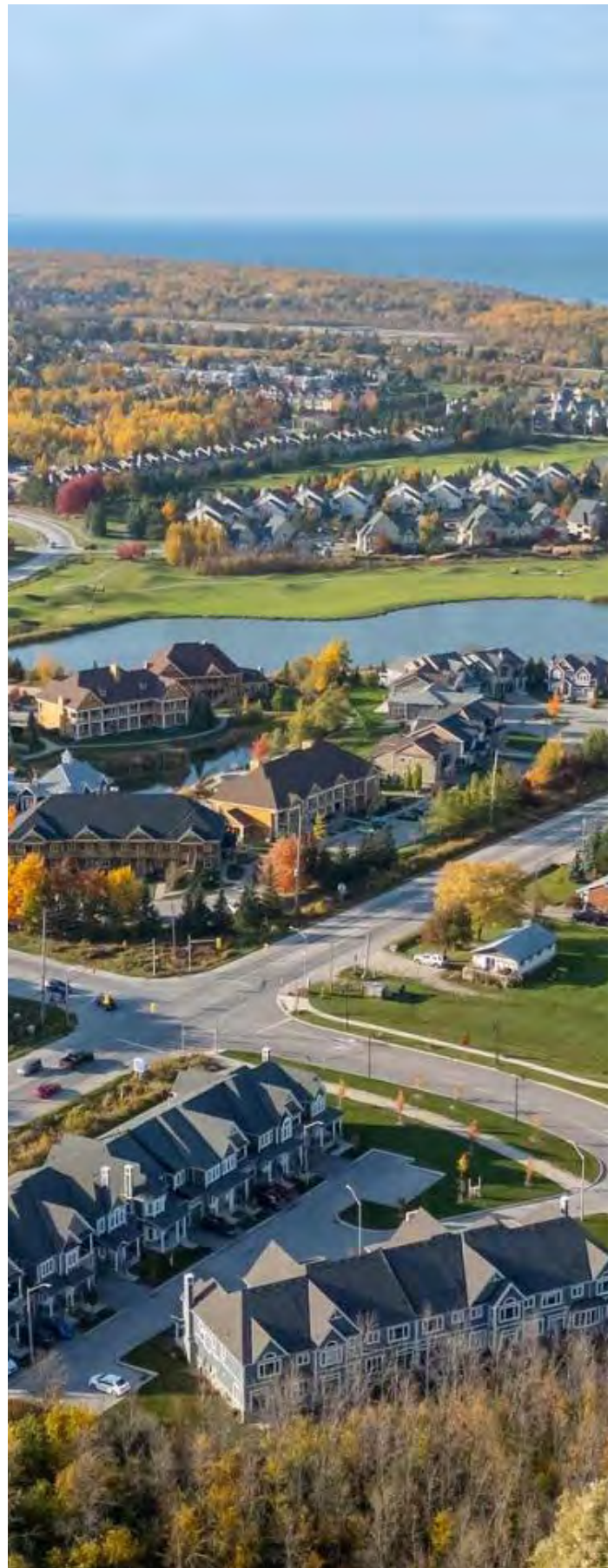
# 1.4

## How to Use the Design Guidelines

The Design Guidelines apply to both the public and private realm and provide specific direction for the design of sites, streetscapes, open spaces and buildings. The Design Guidelines are to be used as a guidance tool during the planning and design process and will be applied during the development review process on a case-by-case basis. The Design Guidelines are not intended to be used as “policies” or “regulations and will be applied in a flexible manner that allows for creativity in design or alternative approaches, while still ensuring the intended vision for development across the Town’s communities is met. The guidelines will be interpreted and applied flexibly, with action words serving to indicate the Town’s ultimate objectives rather than prescribe rigid requirements.

The Design Guidelines should be read and used in conjunction with the Town’s policies, by-laws, plans and initiatives including:

- The Blue Mountains Official Plan
- Comprehensive Zoning By-law
- Engineering Standards
- Community Sustainability Plan: The Blue Mountain’s Future Story
- Leisure Activities Plan
- Transportation Master Plan
- Town-Wide Revitalization Community Improvement Plan
- Municipal Tree Preservation By-law



## 1.4.1 Implementation

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The Design Guidelines will be implemented through the development review process. The Town may require submission of a Community Design Checklist (see **Appendix A**) and supporting materials or studies to confirm that the applicable Design Guidelines have been considered and incorporated into development proposals.

Applicants should reference both **Volume 1** and **Volume 2** when preparing development proposals. Development should demonstrate how the Guidelines in **Volume 1** have been applied to the design of buildings, sites, and the public realm, while responding to the surrounding context and locational characteristics described in **Volume 2**. Proposals should clearly identify how the design supports the vision for the community in which the site is located and contributes to a strong and cohesive sense of place.

The following sections of **Volume 1** apply based on location and context:

### Thornbury and Clarksburg

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#### Residential Neighbourhoods

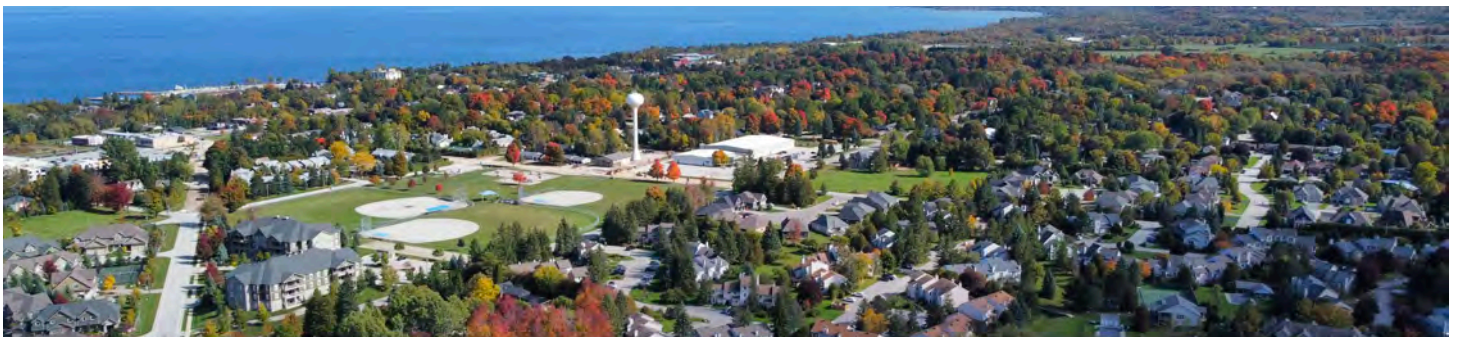
- Section 2 - Environment, Parks, Public Spaces and Culture
- Section 3 - Neighbourhood Design and Residential Built Form

#### Downtown Thornbury and Clarksburg

- Section 4 - Downtown and Commercial Areas
- Section 4.1 - Downtown Streetscapes
- Section 4.2 - Built Form
  - Section 4.2.1 - Scale, Orientation and Siting - *Downtown Thornbury/Clarksburg*
  - Section 4.2.2 - Character and Design - *Downtown Thornbury/Clarksburg*
  - Section 4.2.3 - Entrances and Openings - *Downtown Thornbury/Clarksburg*

#### Highway 26 Through Thornbury

- Section 4 - Downtown and Commercial Areas
  - Section 4.2.1 - Scale, Orientation and Siting - *Highway 26 Through Thornbury*
- Section 5 - Urban Employment Area



## Craigleith

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### Residential Neighbourhoods

- Section 2 - Environment, Parks, Public Spaces and Culture
- Section 3 - Neighbourhood Design and Residential Built Form

### Craigleith Village

- Section 4 - Downtown and Commercial Areas

## Lora Bay

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### Residential Neighbourhoods

- Section 2 - Environment, Parks, Public Spaces and Culture
- Section 3 - Neighbourhood Design and Residential Built Form

## Camperdown

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### Residential Neighbourhoods

- Section 2 - Environment, Parks, Public Spaces and Culture
- Section 3 - Neighbourhood Design and Residential Built Form

Refer to Volume 2 of the Design Guidelines for additional context on Town's community structure and area-specific objectives that will guide development and design within each community.

## 1.4.2 Sustainable Design

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Sustainability is a key component of good design and has been integrated throughout the Community Design Guidelines to support the Town's objectives for climate resilience, environmental protection, energy efficiency, sustainable transportation choices and healthy communities.

Sustainability-focused guidelines and metrics are included throughout the Guidelines, where applicable to different types of development, buildings and public realm conditions. These measures are identified with a leaf symbol to highlight opportunities to advance more sustainable design and development outcomes.



## 1.5 Character, Compatibility and Sense of Place

Character, compatibility and sense of place are important considerations in the design of development; however, these concepts are inherently subjective. Character and compatibility are defined terms in the Official Plan that help guide how development should relate to its surroundings. Within the Design Guidelines, these concepts are translated into practical design direction that shapes how development looks, functions and fits within the community.

Compatibility should be understood as the ability of new development to fit well within its surroundings through appropriate scale, massing, site layout and its relationship to the public realm. It does not mean mimicking what already exists; rather, development should respond to its context in a way that contributes positively to the area and supports its ongoing evolution. These considerations are intended to guide design and are not applied as rigid rules. The Guidelines do not promote a singular design style, but instead support high-quality design that responds to context through thoughtful building design, site organization, landscaping and attention to the public realm.

A strong sense of place is created when development contributes positively to its surroundings and helps make an area recognizable and meaningful. It comes from the relationship between buildings, landscapes, public spaces, and the activities they support. Development should build on the positive qualities of its context, such as scale, materials, and landscape, while introducing design that enhances the overall experience of the area.

**“Character”** means the collection of distinct features that work together to identify a particular area or neighbourhood, which may include the built and natural elements of an area.

**“Compatible”** means development or redevelopment which may not necessarily be the same as or similar to the existing development, but can coexist with the surrounding area without negative impact.

**“Enhance”** means to complement and strengthen the character of the Town, community, neighbourhood, site or structure.

**“Sense of Place”** refers to the distinctive qualities that make a location recognizable, meaningful and memorable. It emerges from the relationship between built form, landscape, public spaces, cultural features, and the activities that occur there. A strong sense is created by designing buildings, streets, and open spaces that reinforce community identity and support comfortable, vibrant and welcoming places for people.

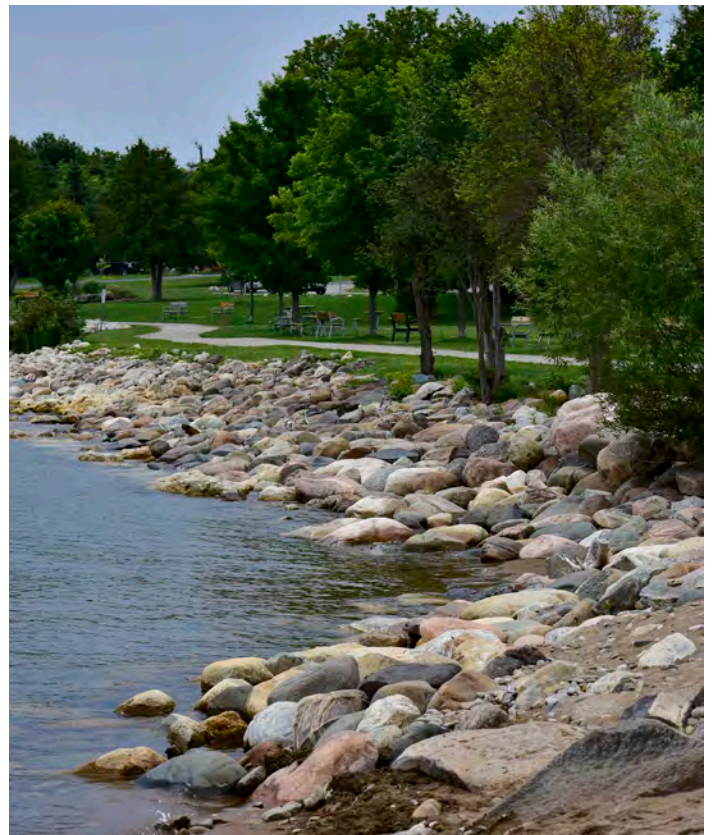
Through each development application, the guidance in this document should be applied in a way that reflects each specific site and its context, with reference to the area-specific objectives in **Volume 2**. This approach supports the creation of a strong and cohesive sense of place across the Town, while allowing for flexibility, innovation, and architectural diversity.

# 2

## Environment, Parks, Public Spaces and Culture

The public realm is comprised of spaces that belong to and are accessible to everyone, including natural areas, parks and open spaces, trails and public or heritage buildings. Elements of private development can also influence how people experience and connect with the community, including cultural heritage resources. Public spaces and elements within those spaces should be carefully designed in response to their context to provide opportunities for community life, social interaction, enjoyment of the environment, recreation, entertainment and reflection.

This Section of the Design Guidelines provides direction for development around the natural environment, parks and open spaces, trails, community spaces and public buildings, public art, cultural heritage buildings and gateway features.



## 2.1 Natural Environment






The Town's interconnected system of natural features and open spaces contributes to the overall health and character of the community. Through all new development and redevelopment, there is a collective responsibility to protect and enhance natural heritage features and functions throughout the Town to ensure natural beauty and amenities are maintained for future generations



Scenic Caves Nature Adventures

### 2.1.1 Woodlands

#### Guidelines:





-  1. Incorporate woodlands or stands of trees into the fabric of street and block layouts and site design, wherever possible.
-  2. Use appropriate buffers to woodlands to ensure their ecological protection. Consider plantings such buffers with complementary native species to enhance their function and to prevent invasive species from being established.
-  3. Encourage woodlands to have a minimum, 50% public frontage and visibility, through such means as single-loaded streets or incorporation into a park or stormwater management area.
-  4. Where appropriate, incorporate trails within woodlands to connect to the neighbourhood pedestrian network. Trail design should prioritize user safety and environmental integrity.
-  5. Design trails to be consistent with the character of woodland features.



Incorporate existing trees and woodlots into the fabric of street, block and site layout wherever possible

## 2.1.2 Watercourses

### Guidelines:

-  1. Maintain and enhance vegetation, and encourage regeneration, to every extent possible within watercourse corridors.
-  2. Naturalize watercourse corridors with native species to promote the habitat and aesthetic characteristics of such features.
-  3. Incorporate linear pedestrian pathways along watercourse corridors while maintaining the environmental sensitivity of such features.
-  4. Design pathways to be consistent with the character of watercourse features, preferably using permeable natural materials to permit infiltration.







Firemans Park



Clendenan Dam

## 2.1.3 Trees and Hedgerows

### Guidelines:

-  1. Ensure tree protection and preservation in accordance with the Town's Municipal Tree Preservation By-law.
-  2. Locate street and development blocks to incorporate existing quality hedgerows into side or rear lot lines or along linear pedestrian corridors.
-  3. Incorporate existing healthy trees within building lots by varying building setbacks and varying building design.
-  4. Physically protect any trees to be retained during construction and grading to avoid disruption to their roots.




Ensure tree protection and preservation in the location of streets and development blocks

## 2.2 Parks, Open Space and Waterfront Areas

Parks and open spaces are a valuable asset in The Blue Mountains. Throughout the Town, parks and open spaces contribute to the physical and mental well-being of both residents and visitors, as well as overall community sustainability, culture and diversity. Parks and open spaces require a design that is safe, secure, accessible, age-friendly and inclusive and capitalizes on the natural and cultural heritage character of its surroundings.

### Guidelines:





1.  Ensure all parks are connected to the Town's overall network system of parks, natural areas, the waterfront and stormwater management ponds through sidewalks, trails, pathways, or mid-block walkways that accommodate both pedestrians and cyclists.
2. Incorporate a variety of active and passive uses within parks, both structured and unstructured, to accommodate the needs of all users and facilitate a number of functions including children's play, cultural gatherings, socializing and special events.
3. Integrate, where appropriate, elements such as seating, hard surface areas, shaded areas, open air structures, public art, pedestrian-scaled lighting and distinctive tree, shrub and ground cover planting.
4. Design play spaces that are welcoming and inclusive for people of all ages and abilities, including children, older adults caregivers and people with disabilities.
5. Locate park entrances at the intersection of two streets where possible, to encourage safe pedestrian access.
6. Define main entrances with soft and hard treatments, signage and sitting areas with appropriate amenities.



Heritage Park

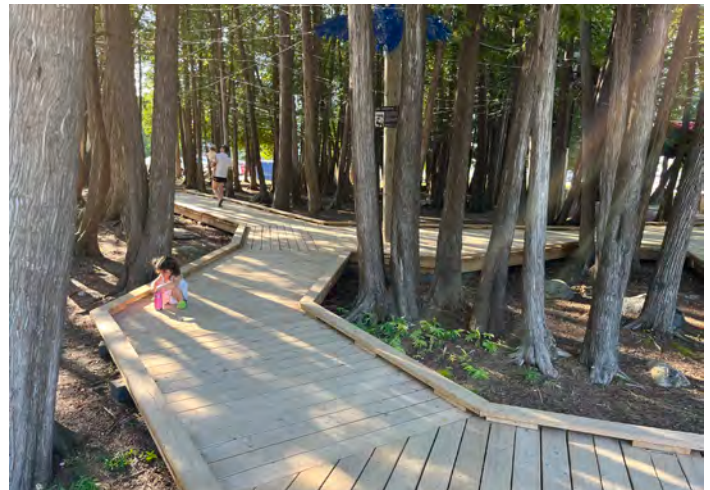


Integrate a variety of active and passive uses to facilitate the needs of all ages and abilities

7. Ensure a majority of the park's frontage is open to the street, maximizing visibility from adjacent streets and promoting safety.
8.  Protect, maintain and enhance views and vistas to natural areas, Georgian Bay and open spaces through siting and design.
9.  Prioritize sustainability in park design through material choice, low impact development (LID) measures and appropriate landscaping and vegetation. LID measures may include rain gardens, bioswales and permeable surfaces to support stormwater management and ecological function.
10. Design pathways to enhance the function and character of the type of the park they occupy, keeping in mind user safety, lighting and intended operational hours.
11. Use plantings and grading to define the boundaries of a park, reduce the impacts of noise, provide a visual buffer between adjacent development and prioritize Crime Prevention Through Environmental Design (CPTED).
12.  For parks adjacent to natural areas, consider appropriate interface buffers, signage and trail access. Limit the use of natural areas to passive and low-intensity recreation uses, trails and lookouts.
13. Design parks to minimize any potential negative impacts on adjacent residential areas through the use of planting, fencing and/or the provision of appropriate access, parking and buffers to active recreational facilities;
14.  Preserve existing significant trees and vegetation wherever possible and incorporate them into park design.







Thornbury Harbour



Design pathways to enhance the function and character of parks



Plantings and furniture help to define park character and contribute to Crime Prevention Through Environmental Design (CPTED)

- 15.  Provide and locate trees within parks and public open spaces to create favourable micro climate conditions, such as providing shade and also mitigate wind impacts.
- 16.  Select and arrange trees and other plantings to create efficiencies in maintenance and watering.
- 17.  Prioritize the use of native over non-invasive plant species.
- 18.  Provide bicycle storage facilities within all public parks and open spaces to encourage cycling as a viable mode of transport or recreation activity.



Preserve and maintain existing trees and provide additional trees along pathways to create a favourable micro climate

## 2.2.1 Town-Wide Parks

### Guidelines:

- 1. Town-wide parks are designed to generally service the entire Town and visitors.
- 2. Town-wide parks should have a minimum area of six (6) hectares of primarily tableland.
- 3. Locate access to town-wide parks from Provincial Highways or County Roads.
- 4. Design town-wide parks to provide passive recreational areas (e.g. woodlots, natural areas and open space) and/or active recreational areas including major athletic facilities or standard sports fields, hard surface play areas, special event staging areas, and other major recreation facilities with associated play, water play, washrooms, pavilions or service areas.



Tomahawk Recreation Complex



Provide a mix of passive and active recreational areas

## 2.2.2 Community Parks

### Guidelines:

1. Design community parks to generally service a Settlement Area or Hamlet Area.
2. Generally community parks have a minimum area of four (4) hectares of primarily tableland.
3. Locate community parks with access and frontage along a County Road or Collector Road.
4. Wherever possible, locate community parks adjacent to a school or community facility, such as a community centre, arena, pool or library to provide a local focal point.
5. Design community parks to be the site of primarily outdoor recreation uses with broader community facilities such as water play areas, gardens or special event staging areas with associated washrooms, pavilions or service areas.



Moreau Park



Incorporate outdoor recreation uses such as play areas, washrooms, pavilions and gathering spaces



Design community parks to provide a local focal point with a broad mix of community facilities

## 2.2.3 Neighbourhood Parks

### Guidelines:

1. Design neighbourhood parks to service the immediate neighbourhood or residential area, generally servicing lands within a 400 to 800 metre radius.
2. Generally neighbourhood parks range in size from 0.5 to 4 hectares, depending on population density and parkland need.
3. Locate neighbourhood parks central to the neighbourhood or residential area they are intended to serve.
4. Locate neighbourhood parks with frontage on a Collector Road that is adequate for the provision of on-street parking and visibility into the park.
5. Where feasible, locate neighbourhood parks adjacent to schools, other community facilities, open space areas or stormwater management facilities to compliment existing facilities and provide a neighbourhood focal point.



Dog park at Heritage Park



Pickeball courts and Tomahawk Park



Locate neighbourhood parks central to the residential area they serve and provide a range of active and passive outdoor uses

## 2.2.4 Parkettes

### Guidelines:

1. Design parkettes to service the immediate neighbourhood or residential area, generally servicing lands within a 200 to 400 metre radius.
2. Generally parkettes range in size from 0.2 to 0.5 hectares, depending on population density and parkland need.
3. Locate parkettes central to the neighbourhood or residential area they are intended to serve.
4. Provide a range of opportunities for active and passive activities, particularly for young children and older adults seeking close to home activities.



Provide connections to parkettes in close proximity to dwellings



Provide a range of opportunities for active and passive activities, particularly for young children and older adults

## 2.3 Trails




The Town will continue to establish a diverse network of trails to provide pedestrians, cyclists, skiers and other non-motorized transportation types more opportunities for connecting to key destinations and points of interest throughout the The Blue Mountains. The trail network will facilitate access between the major recreational activities associated with the Niagara Escarpment and Georgian Bay, as well as the major residential and commercial community areas of the Town.

Primary linkages for the overall trail network are identified in the Town's Official Plan, which should be provided for in the design of development, always ensuring that the general orientation of pedestrian access is maintained. Particular attention should be given to the establishment of an open space corridor along the Nipissing Ridge, with linkages to support the integration and enhancement of the Bruce Trail and Georgian Trail





Georgian Trail

### Guidelines:

-  Design the overall trail network to include a variety of trail types throughout the Town to provide multiple options for trail users, including nature trails, soft surface, hard surface, on-road, boulevard and other types as applicable.
-  Incorporate an extensive network of open space public trails as part of subdivision design, in addition to sidewalks along roadways.
-  Ensure accessible parking, entrances, paths and lookout points designed for people of all ages and abilities.



Provide multiple trail options for users of all types




- 4.  Improve and expand trail linkages wherever possible to provide public access to the waterfront.
- 5. Integrate a coordinated and comprehensive system of wayfinding across the trail network to clearly mark where public access is located and enhance wayfinding signage wherever required.
- 6.  Incorporate and maintain bicycle racks, seating, rest areas, shade trees and vegetation along the trail network, particularly at main entrances.



Incorporate a variety of amenities at main trail entrances

## Georgian Trail

The Georgian Trail is a key component of the inter-municipal trail network and a major tourist attraction. Development adjacent to the Georgian Trail should protect the continuity and character of the corridor while enhancing the experience for trail users.

- 7.  Maintain the Georgian Trail as a continuous and protected corridor by limiting new road crossings and trail connections to strategic locations only.
- 8.  Consolidate connections from adjacent development areas to minimize interruptions along the trail.
- 9.  Incorporate design measures such as landscaped buffers, additional tree planting, and vegetation to screen views of adjacent development from the trail and enhance the experience for trail users.



Define trail gateways with signage



Provide seating, landscaping and small amenities along the trail to enhance comfort and the overall trail user experience

## 2.4 Stormwater Management

Stormwater management facilities, such as ponds and channels, should be designed to integrate into their surrounding development context and open space system, rather than located as isolated utility elements at the periphery of development. Stormwater management infrastructure should be designed to function as “green amenities” contributing to the visual character and ecological function within a neighbourhood. Their design should consider overall aesthetics, landscape integration and opportunities to support wildlife habitat and naturalized planting.

For detailed design, refer to the **Town’s By-law to Regulate and Prohibit Activity in and Around Stormwater Management Facilities.**

### Guidelines:

1. Integrate stormwater management ponds within the neighbourhood open space network and provide connections through pedestrian and cyclist linkages.
2. Design stormwater management ponds with majority of their frontage open to public streets to enable visual and physical connections.
3. Design stormwater management ponds and any adjacent parks in a consistent manner to provide a visually continuous green space.
4. Use native species as naturalized edges to the water body portion of the stormwater management area instead of fencing to deter public access to such areas.
5. Where fencing is required for safety reasons, use decorative fencing that complements the natural character of the stormwater management area.
6. Incorporate amenities such as benches, garbage receptacles, information boards and lookout areas to the complement passive recreation use.



Create naturalized edges to the water portion of stormwater management areas



Provide pedestrian and cyclist linkages and amenities around stormwater management ponds





Design stormwater management ponds with majority of their frontage open to public view

## 2.5 Community Spaces and Public Buildings

Community facilities and public buildings play an important role as a community precedent for high-quality sustainable site and building design. Public buildings, such as Town Hall, community centres, arenas and libraries, serve as important landmarks, gathering spaces and amenity areas for residents and visitors.

### Guidelines:

1. Design new or enhance existing public buildings with distinct and prominent architecture.
2. Design the building to fit within the existing context and specific character, including adjacent land uses and surrounding building types.
3.  Integrate sustainable design features, such as low impact development as part of the building and site design.
4. Integrate accessible public spaces at main entrances and within the site, in accordance with Crime Prevention Through Environmental Design (CPTED) principles.
5. Design the front door to face the main street and be directly accessible to the public sidewalk.
6. Locate parking at the side or rear of the building, wherever possible.
7. Include landscaped buffers, clear pedestrian pathways to building entrances and landscaped islands throughout the parking area.
8.  Encourage energy-efficient building design, including the use of recognized sustainability standards such as LEED or similar certification programs.



The Blue Mountains Town Hall



Design public buildings with distinct and prominent architecture and landscaping



Landscaping and amenities incorporate outside Town Hall

## 2.6 Public Art

Culture, creativity and diversity play a significant role in the success of the community and future development. Public art is encouraged throughout the Town as a means to foster community and neighbourhood identity and contribute to a vibrant and creative Town.

### Guidelines:

1. Encourage public art to take a range of sizes, artistic mediums (metal, stone, paint), and variety of different forms (architectural features, sculptures, landscape features, street amenities, murals, infrastructure or paintings).
2. Ensure that public art is accessible and visible to members of the public, located either within a public street right-of-way or other publicly owned space, or on a private property where it has an interface and connection with the public realm.
3. Incorporate public art into site design and/or into building design as an element of the architectural design. At the initial stages of development where no public art is provided, installation sites should be included for future pieces.
4. Encourage public art installations that may serve a combination of different purposes, such as functional, interpretive, abstract, or historical.
5. All park spaces should consider including opportunities for public art. Along the waterfront, public art should feature stand-alone pieces or installations, as well as artistic elements incorporated into infrastructure such as public benches.



Encourage public art to take a range of sizes, materials and forms



Consider public art installations that are directly integrated into the public realm and reflect surrounding character

6. Aesthetic treatments of infrastructural elements is encouraged in downtown areas, which could include banners on sign poles, vinyl wraps on utility cabinets, concrete designs on bridges, specific colours on bridges and/or other steel structures.
7. Locate interactive public art installations in popular public spaces and downtown areas.
8. Ensure public art has the primary function of providing visual interest without any commercial advertising function.
9. Locate any public art to limit any conflicts with vehicular, bicycle or pedestrian circulation.
10. Ensure public art is durable and easy to maintain.
11. Ensure the community is involved in the design of public art and murals as much as possible.



Public art at Hester Street Parkette



Encourage a variety of public art forms that serve various purposes, including aesthetic and educational

## Murals

12. Ensure that any murals are selected by the Blue Mountains Library Board, completed by a qualified professional artist and are not commercial advertising of any sort.
13. Use themes that are related to the particular community's history, tourism, features, character or any artistic expression.
14. Locate murals only on exterior walls that do not contain the primary building entrance.
15. Design murals so that they are consistent with the building style and do not obscure the building's architectural details.
16. Use high quality, durable, graffiti-resistant, and weather resistant materials for murals.
17. Light murals, where appropriate, with fixtures consistent with the primary building lighting to provide emphasis at night.



Design murals to be consistent with the building style and do not obscure any architectural details.

## 2.7 Cultural Heritage Buildings

Cultural heritage resources throughout the Town greatly contribute to the overall small-town and rural fabric of communities, attract tourism and enhance the character and vitality of neighbourhoods and distinct areas. It is a priority of the Town to protect and maintain cultural heritage resources and preserve the Town's rich heritage for future generations.

Heritage properties, both Designated and Listed, are valuable assets that embody the historical, cultural and architectural legacy of the community. Preserving these properties requires a thoughtful and sensitive approach to ensure that any modifications or developments respect their unique character and significance.

### 2.7.1 Renovations to Designated and Listed Heritage Properties

#### Guidelines:

#### Roofs and Accessory Elements

1. Conserve original materials, configuration and accessories that are historically associated with the architectural style of the building such as chimneys, projections, building elements extending above the roofline, etc.
2. The restoration of damaged components is preferred prior to replacement. If unavoidable, original material should be replaced only in heavily damaged areas with compatible materials.
3. Restore roofing with original materials based on archival evidence in cases where existing roof is not original and requires replacement.



Craigleith Heritage Depot



Restore roofing with materials consistent to the historic architectural style

## Exterior Cladding

4. Retain and conserve original exterior cladding wherever possible.
5. The repair of damaged components is preferred prior to replacement. Replacement should only be done in areas of heavy deterioration. In cases where replacement is required, in-kind materials should be used which conform to, or simulate the form, colour and profile of the original materials.

## Windows and Doors

6. Conserve windows and doors, as the key defining elements of an architectural style.
7. Repairs are preferred wherever possible. Proper repairs should be made depending on the specific type of window.
8. Replacement with in-kind window materials is preferred when historic windows and doors are deteriorated beyond repair. Replicas may be installed if they have the same material, style and proportions as the original style.
9. If thermal performance is in need of improvement, consider retrofitting or use of storm windows and doors as appropriate.

## Porches and Verandas

10. Porches and verandas must be conserved. Repair is preferred over replacement. If an element has deteriorated beyond repair, it should be replaced with a replica element.
11. Removals, additions to and/or enclosure of porches are not supported as they alter the architectural style of the principal facade of the building.
12. Reinstating original porches from archival evidence is strongly encouraged.
13. Non-original porches may be added to the structure at the side or rear of the building away from the principal building facade.



Exterior cladding materials should simulate the form, colour and profile of original materials



Repair or replace windows and doors with in-kind materials as key defining architectural elements



Preserve and repair porches based on their original architectural style

## 2.7.2 Additions to Heritage Properties

### Guidelines:

1. Limit the scale of additions to avoid overwhelming a heritage property's appearance.
2. Design (colours, materials, scale, proportions, etc.) should be respectful and sympathetic towards the original structure.
3. Incorporate universal accessibility without severely affecting the character-defining elements of the property.
4. New windows or doors should be inconspicuous and not located along the principal building façade.
5. Additions should be located at the rear or along an inconspicuous side of a historic building.



Colours, material and scale should compliment the existing structure



New design elements should be sympathetic and respectful towards the original structure

## 2.7.3 New Development Adjacent to Heritage Properties

### Guidelines:

1. New buildings should not falsify history or attempt to copy materials and styles of historic buildings.
2. New buildings should not obstruct significant views of existing heritage buildings.
3. Access to properties should be in line with adjacent properties (driveway, laneway, etc.).
4. Design cues should be derived from adjacent properties such as materials and colour palettes.
5. Building height and massing should be sympathetic to adjacent properties using appropriate setbacks.





New buildings should not attempt to copy materials, but reinforce the prominence of existing heritage buildings

## 2.8 Gateway Features

Community gateways are intended to achieve a sense of entrance and arrival through public realm design, built form, public art features and landscaping. Through the Gateway and Wayfinding Signage Project, the Town will implement a uniform sign program that will instill a strong sense of place and establish a clear and identifiable hierarchy of signage.

### Guidelines:

1. Gateway features should combine artistic sculptural elements, topographic features, planting, accent lighting and signage, as appropriate.
2. Ensure high-quality, distinctive design of buildings at gateway locations that contributes to the identity of the particular gateway area.
3. Design features to relate to the street width, building massing and open space area. Features should be legible at the pedestrian and vehicular scale and speed of movement, as applicable.
4. Use one style of gateway feature for the overall system in downtown areas that allows all to read as Town-wide system, but which accommodates the uniqueness of each downtown area through special design elements.
5. Use local materials for gateway features that reflect the character of the area.
6.  Emphasize gateway features with surrounding planting material that is native, non-invasive, low maintenance, salt tolerant, and suited to the soil conditions.
7. Use materials and elements that are durable and easily maintained.
8. Use simple and universally readable lettering.
9.  Consider energy-efficient forms of lighting to highlight the gateway features at night.



The Blue Mountains welcome sign



Gateway features should combine signage, lighting and landscaping



Emphasize gateway features with surrounding planting material suited to existing soil conditions

# 3

## Neighbourhood Design and Residential Built Form

The Blue Mountains comprises a variety of unique residential areas and neighbourhoods, which vary considerably in location, layout, lot characteristics, building and architectural styles, topographic attributes, vegetation and tree canopy, open space areas and heritage features. Existing residential neighbourhoods across Thornbury, Clarksburg, Craigleith, Camperdown, Lora Bay and in between will retain their existing character while also allowing for change and growth over time.

A broad range of dwelling types from single-detached and semi-detached dwellings to townhouses and low-rise apartments up to three storeys are encouraged in appropriate locations across all residential areas of the Town.

Thoughtfully designed residential development and redevelopment across the Town will enhance the vibrancy and diversity of neighbourhoods, revitalize underutilized lands and support the Town's vision of providing for a range of housing options. Residential growth is expected to occur within the Town's existing neighbourhoods through infill and redevelopment, as well as through development in new greenfield areas. Greenfields refers to lands within the Town's existing Settlement Area boundaries that have not yet been fully developed.

This Section of the Design Guidelines provides direction for neighbourhood design, residential built form, infill and intensification.

## 3.1 Neighbourhood Design

Within the boundaries of the Town's existing Settlement Areas, opportunities remain for new residential neighbourhood development. New neighbourhoods should be developed to provide for a range of dwelling types and densities through high-quality design that focuses on connectivity, diversity, open space, natural features and sustainability.

### 3.1.1 Streets and Blocks

Streets and blocks form the foundation of neighbourhood design, shaping the flow of movement, accessibility and overall character. Well-designed streets and blocks enhance the quality of life for residents by promoting walkability, connectivity and access to open spaces and amenities.

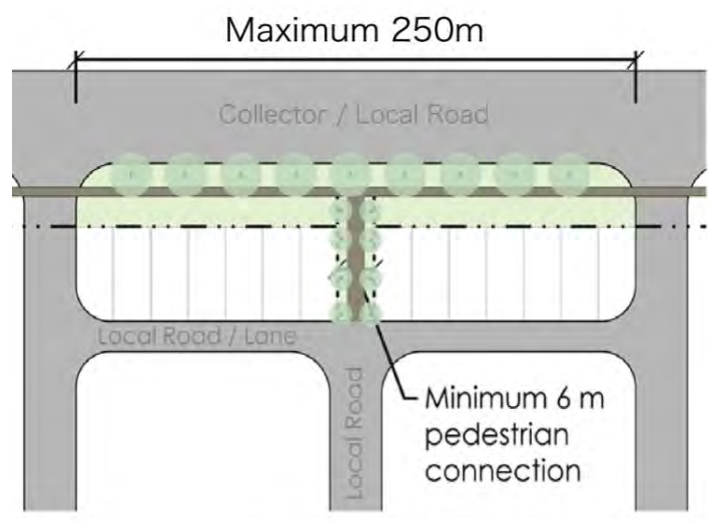
#### Guidelines:

1. Incorporate landform features and topography in the design of street and block patterns to maximize vistas and visual interest and to reduce the need for grade alteration or soil movement. Irregular shaped blocks are appropriate when responding to topographic or unique conditions or to achieve a distinct neighbourhood character.
2. Design the street pattern so that development blocks do not exceed 250 metres in length for Local Roads to support walkability and connectivity.
3. Provide mid-block connections or walkways at least 6 metres wide along Collector Roads or Highway 26 where blocks lengths may need to be slightly longer than 250 metres.




For residential streetscape design, stormwater management, lighting, utility coordination and other elements refer to the **Town's Engineering Standards**.



Design block patterns to maximize views and vistas and provide visual interest to a neighbourhood



Design the street pattern so that development blocks do not exceed 250 metres in length and provide mid-block connections where blocks may be slightly longer

4. Size and design blocks to allow for a variety of dwelling types and accommodate adequate setbacks, outdoor amenity spaces and parking areas.
5.  Design or modify the street network to incorporate any existing natural features such as woodlots, watercourses, hedgerows or significant trees and vegetation wherever possible.
6. Design or modify the street network to retain, protect and enhance significant cultural heritage resources.
7. Where appropriate, orient streets to terminate at or provide views toward publicly accessible spaces such as parks, trails, natural heritage features or landmark buildings.
8. Adjacent to natural features, parks and open spaces, roads should be single-loaded to define the edge and provide access and visibility into open spaces. Design public parks with a minimum 50% public road frontage.
9. Layout street patterns to maximize the number of blocks with south facing exposures for increased solar orientation.
10.  Ensure development parcels provide sufficient public and private landscaped open space to support stormwater infiltration, retention and runoff management through parks, natural features, stormwater management facilities and on-site landscaped areas.
11.  Ensure sidewalks and cycle paths are accessible, safe, well lit, and protected. Sidewalks should be separated, or designated differently, from automobile traffic to ensure safety.



Size and design blocks to allow for adequate setbacks, trees and parking



Orient streets to emphasize the location of adjacent trails and natural features




Design public parks to have a minimum 50% public road frontage

### 3.1.2 Lot Configuration and Building Orientation

The orientation of buildings and the layout of residential lots has a significant impact on the functionality, sustainability and aesthetic quality of a neighbourhood. Well-configured lots can maximize natural light, enhance energy efficiency and enhance open space character.

#### Guidelines:

1. Design new lots with a similar lot coverage and setbacks to ensure the scale of development and open space reflects a contiguous streetscape appearance.
2. Site buildings parallel to the street, where possible, with the primary building façade directly relating to the street.
3. Arrange buildings to frame views and vistas to parks, recreational spaces such as ski clubs and golf courses, natural heritage features including the Niagara Escarpment and Georgian Bay and/or heritage buildings.
4. Locate prominent community buildings or uses at key locations, such as the termination of streets or at neighbourhood gateways.
5. Orient buildings and lots to maximize solar access and opportunities for passive solar design.
6.  Design lot layout and building placement to support natural ventilation, tree preservation and opportunities for street trees, permeable surfaces, bioswales or rain gardens.



Arrange buildings to frame views and vistas to parks and open spaces



Design new lots with a similar lot coverage and setbacks to reflect a contiguous streetscape appearance



Ensure buildings are sited parallel to the street, with the primary building facade directly relating to the street

## 3.2 Residential Built Form

All residential built form in the Town should be compatible with its setting, whether in a more established built-up area or rural context, and consistently designed with thoughtful massing, heights, setbacks and architectural details.

Single detached and semi-detached dwellings currently make up a majority of the Town's building stock. In keeping with the Town's Official Plan, new residential development should ensure a diverse range of housing types are accommodated across the Town, which includes single-person households, employees, families and seniors. There are opportunities throughout the Town for new residential development, infill and intensification in appropriate locations that is in keeping with the unique small town feel and character of the Town.

This section of the Design Guidelines provides direction for a range of permitted residential built form types across the Town. These guidelines are intended to provide general design direction, while specific numerical standards, dimensions and performance requirements are set out in the Zoning By-law.



### **Residential Built Form Types:**

- Single Detached Dwellings
- Semi-Detached Dwellings
- Townhouses
  - *Street Townhouses*
  - *Stacked Townhouses*
  - *Back to Back Townhouses*
- Additional Residential Units
- Converted Dwellings
- Low-Rise Apartment Buildings

## 3.2.1 Single Detached and Semi-Detached Dwellings

**Single detached and semi-detached dwellings** are permitted across the Town's residential designated areas including the Community Living Area in Thornbury/Clarksburg and Residential/Recreational Areas. The maximum permitted height of single detached or semi-detached dwellings is two and a half (2.5) storeys.

### Guidelines:

#### Orientation, Massing and Articulation

1. Vary dwelling types and forms on each street and within each block.
2. Vary building elevations and design elements such as material and colour along each street and within each block.
3. Ensure architectural styles are consistent with the overall character of each individual neighbourhood or residential area.
4. Use a variety of building materials and architectural elements on the front, side and roofline of buildings.
5. Site dwellings to ensure the primary façade and front door face the street.
6. Design buildings on corner lots so that the side of the dwelling is treated similar to the front façade in terms of continuity of materials, colour, and window placement.
7. Design corner units to emphasize their priority location and provide design features such as wrap-around porches.
8. Incorporate usable porches as part of the building design, a minimum of 1.5 metres wide, to accommodate furnishings. For corner lots, wrap around porches are strongly encouraged.



Vary building elevations and design elements along each street



Use a variety of building materials and architectural elements while ensuring consistency with existing character



Design buildings on corner lots so that both the front and side are oriented to the public street and incorporate usable porches as part of the building design

9. Site dwellings on a lot to maximize protection of existing mature trees and provide opportunities for new trees to be planted.
10. Design rooflines with varied heights and styles to de-emphasize the overall height of buildings and provide variation.

### ***Driveways and Garages***

1. Design dwellings so that a garage does not project beyond the main front wall or in front of a porch to reduce the visual prominence of the garage.
2. Design dwellings so that garages occupy no more than 50% of the front façade's width and the width of a driveway is no larger than the width of a garage.
3. Design garages to be unobtrusive and have consideration for massing, orientation and architectural details.
4. Design side or rear detached garages to reflect the architectural style of the main dwelling.
5. Orient the driveway and garage for corner buildings to the street that is secondary in terms of traffic volume and function.



Ensure garages do not project beyond the main wall or in front of a porch to reduce visual prominence



Design garages to be unobtrusive and have consideration for massing, orientation and architectural details



Design rooflines with varied styles along a block to provide variation

## 3.2.2 Townhouse Dwellings

**Townhouse dwellings** are permitted across the Town's residential designated areas including the Community Living Area in Thornbury/ Clarksburg and Residential/Recreational Areas. The maximum permitted height of townhouse dwellings is three (3) storeys, or eleven (11) metres, which may take the form of street, stacked or back-to-back townhouses.

### Guidelines:

#### Orientation, Massing and Articulation

1. Townhouse blocks should generally be a maximum of 6 units to provide space between blocks and minimize building mass.
2. Locate townhouse blocks close to the property line with their primary façade and/or entrance addressing the street, while making room for trees and utilities.
3. Ensure that architectural styles employed in townhouse design are comparable with the character of the surrounding area and neighbourhood context.
4. Design each townhouse block with a variety of different features and treatments, including variations in colour, materials, articulation and windows.
5. Vary the elevation types along a street to enhance the visual interest and variety of the streetscape.
6. Design corner units so that the side of the dwelling is treated similar to the front façade in terms of continuity of materials, colour, and window placement.
7. Design corner units to emphasize their priority location and provide design features such as wrap-around porches.



Locate townhouse blocks close to the property line with their primary façade and/or entrance addressing the street



Vary elevation types along a street to enhance visual interest and variety of the streetscape



Design corner units to emphasize their priority location

8. Use a variety of roofline types along an individual block and along the entire street to enhance the visual interest and variety of the streetscape.
9. Incorporate functional porches, which can accommodate sitting areas, into the building design to provide interaction space between dwellings and the street.
10. Provide mid-block pedestrian connections at regular intervals between townhouse blocks in the interior of neighbourhoods.



Incorporate a variety of roofline types and provide mid-block pedestrian connections

### ***Driveways and Garages***

11. Design units so that a garage does not project beyond the main front wall or in front of a porch to reduce the visual prominence of the garage.
12. Design dwellings so that garages occupy no more than 50% of the front façade's width and the width of a driveway is no larger than the width of a garage.
13. Where practical, pair driveways to reduce the amount of front paved surface area required and to maximize opportunities for on-street parking and street tree placement.
14. Design rear detached garages to reflect the architectural style of the townhouse block.
15. Orient the driveway and garage for corner units to the street that is secondary in terms of traffic volume and function.



Ensure garages do not project beyond the main wall or in front of a porch and pair driveways where feasible



Orient the driveway and garage for corner units to the secondary street and provide the pedestrian entrance from the main street

## Stacked and Back-to-Back Townhouses

Stacked and back-to-back townhouses are comprised of units that are stacked vertically and/or horizontally with at grade access.

16. Ensure compatibility with existing context, height, massing and materials.
17. Design visible end units to have entrances, windows and architectural detailing to create interest and animate the elevation.
18. Provide barrier-free units that are directly accessible from grade wherever possible.
19. Where below-grade units are proposed, they should be designed to ensure sufficient sunlight is provided by combining a below-grade level with an above-grade level to create a two-level unit or design units as “through-units”. Adequate setbacks and landscaped courts/amenity space should be provided in front of below grade units to enhance solar exposure.
20. Where front integral garages are proposed for back-to-back townhouses, they should be flush or recessed from the mail wall of the dwelling and not occupy more than 50% of the front building width. Tandem garages are encouraged.
21. Provide shared private outdoor amenity spaces for the overall development of an appropriate size, shape, location and siting to maximize visibility and accessibility, with direct access to sunlight and sky views.
22. Common outdoor amenity spaces should be sited and designed as focal points, in the form of courtyards, children’s play areas, shared roof top terraces or plazas.



Design visible end units to have quality architectural detailing



Recess front integral garages for back-to-back townhouses



Provide common outdoor amenity space in the form of courtyards, children’s play areas or shared rooftop terraces

### 3.2.3 Infill Development and Replacement Housing

**Infill development** includes the creation of lot(s) for single detached, semi-detached or townhouse dwellings between existing residential lots. Infill develop can occur through a consolidation of lots or the severance of a larger lot.

**Replacement housing** involves smaller dwellings in the Town being substantially altered or demolished and replaced with a new, larger dwelling through the building permit process and potentially the minor variance process.

Existing residential neighbourhoods across the Town's communities are intended to retain their existing character, while still allowing for evolution over time. New buildings in existing neighbourhoods should be carefully designed to ensure it fits into and reinforces the stability and character of each neighbourhood. While infill and replacement housing does not need to mimic the existing type and density of existing housing, it should be designed to respect the overall lot and built form character of the surrounding area.

Infill development within the Town's existing communities, particularly Thornbury and Clarksburg, will ensure the efficient use of land and existing servicing while contributing to the creation of a more walkable, well-rounded community. Growth and infill development will be limited in the Town's Villages and Hamlets, however new development may occur through replacement housing or alterations to existing structures.



Infill development opportunities in Thornbury



Infill development opportunity in Thornbury



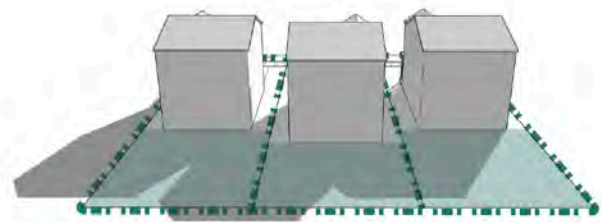
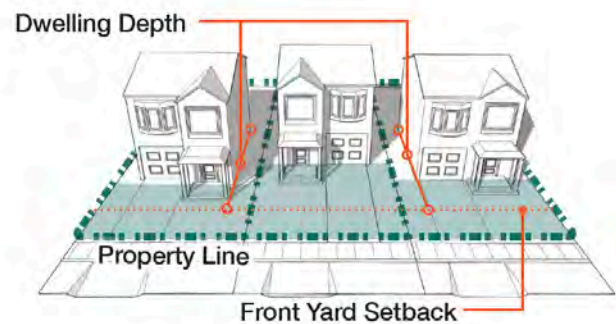
Ensure new buildings are carefully designed to fit into and reinforce existing character

## Guidelines:

1. Ensure the siting, built form and density of new development is sensitive to the character of the existing neighbourhood, including the location, massing and height of adjacent buildings.
2. Design new dwellings and additions to respect or enhance the architectural characteristics of the surrounding neighbourhood through appropriate materials and colours.
3. Incorporate materials and architectural features (e.g. windows, dormers, roofs, etc.) that are consistent in quality of detail and complementary to the existing dwelling or adjacent dwellings.
4. Design the building to reflect the pattern of heights of adjacent dwellings.
5. Provide a similar lot coverage as adjacent housing to ensure the massing or volume of the new dwelling or addition reflects the scale of appearance of adjacent lots.
6. Maintain the predominant front, side and rear yard setbacks along the street to preserve the existing rhythm of the street.
7. Ensure the length and depth of new dwellings is in keeping with the existing dwellings along a street to avoid privacy and overshadow issues.
8. In addition to single detached dwellings, design new semi-detached dwellings and townhouses to respect the existing street pattern and ensure compatibility with single detached dwellings along the block face.
9. Where applicable, ensure main entrances face the street to enhance visibility and natural surveillance.



Incorporate materials and architectural features complimentary to adjacent dwellings



Similar front and rear yard setbacks and dwelling depths



Maintain predominant front and side yard setbacks

10. Ensure the frontages of new interior lots are generally no less than 70% of the average lot frontages on the same side of the public road to provide for, to the greatest extent possible, appropriate separation between new and existing dwellings.
11. Ensure the frontages of new corner lots are generally no less than 80% of the average lot frontages on the same side of the public road to provide for an appropriate setback from the exterior side lot line.
12. Encourage a variety of roof lines and shapes within each residential block while maintaining a consistent scale and height with existing adjacent dwellings for new dwellings and additions.
13. Transition flat roofs to lower dwellings through stepped levels and increased side yard step backs.
14. In neighbourhoods with an established pattern of detached garages located in the rear yard, also locate new garages at the rear of the dwelling, where space permits.
15. Provide soft landscaped areas along public frontages and ensure the minimum landscaped area is maintained in the front and flankage yards in accordance with the Comprehensive Zoning By-law.



Consider a variety of rooflines and building shapes while ensuring consistent scale and height with adjacent dwellings



Consider infill development for adding gentle intensification and a broader mix of housing choices into a neighbourhood



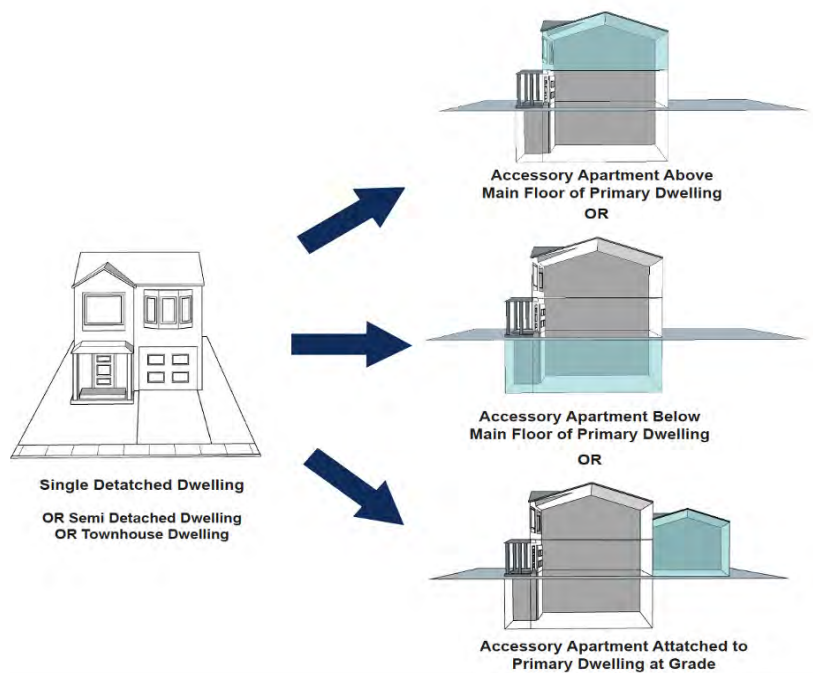
Maintain predominant front and side yard setbacks to preserve the existing rhythm of the street

## 3.2.4 Additional Residential Units

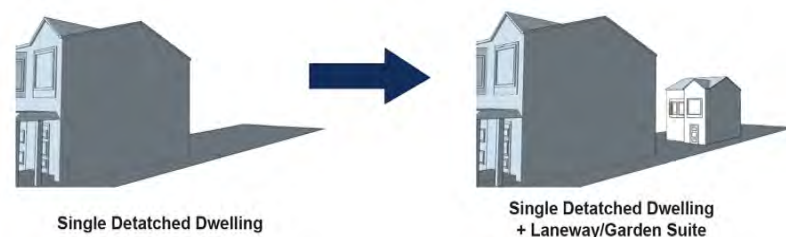
### **Additional residential units (ARUs)**

are permitted across all residential areas of the Town, including the Community Living Area and Residential/Recreational Area designations in accordance with Section B2.7 of the Town's Official Plan. ARUs may be located within existing single detached, semi-detached or townhouse dwellings or may be located within a detached accessory building on the same lot (such as a garden suite, laneway suite or coach house). ARUs can serve as an effective form of gentle intensification within neighbourhoods, making efficient use of the Town's existing infrastructure and contributing to the creation of a complete, compact community.

**Converted dwellings** are typically larger single or semi-detached dwellings that have been internally altered to provide for additional dwellings units beyond the maximum three (3) units per lot permitted for ARUs. Converted dwellings are permitted in the Community Living Area designation in Thornbury/Clarksburg, in accordance with Section B2.9 of the Town's Official Plan. The conversion of existing dwellings to include additional units may increase the number of main entrances, windows and/or parking spaces on site, among other elements. Converted dwellings are specifically encouraged in proximity to community services and amenities including Downtown Thornbury and Clarksburg.



Options for additional residential units in existing dwelling



Additional residential unit exterior to existing dwelling



Converted dwelling example

## Guidelines:

1. Maintain the residential nature of the existing residential building and structures through the use of similar materials, colours and proportions.
2. Ensure there is sufficient space on the lot to provide one parking space for each unit, with parking located in the side or rear yard wherever possible.
3. Units located in the basement of an existing dwelling should be well designed and provide safe access with sturdy materials, lighting and handrails.
4. Provide landscaping and separate outdoor amenity space for each unit such as screened patios and porches.
5. Ensure there is sufficient spacing between two adjacent dwellings without compromising the character of buildings along the road.
6. Where applicable, locate habitable space towards the flanking street and laneway to activate the frontage and encourage casual surveillance.
7. Provide a complementary rhythm, scale, and height to that of the surrounding streetscape. ARUs located above or in place of a laneway garage should not exceed the height of the primary building.
8. Encourage symmetry in building design for all dwellings on the lot by using materials and architectural detailing that are compatible and/or complementary with those of the principal dwelling. Innovative and contemporary architecture provides opportunities for integration through form, materials and scale.



Converted dwelling in Thornbury



Utilize materials and architectural details that are complimentary to the principal dwelling



Provide separate outdoor amenity spaces for each unit

## Detached Additional Residential Units

9. Detached ARUs are permitted in accordance with the Zoning By-law.
10. Ensure the maximum building height of an ARU does not exceed 5 metres, except where the ARU is located above another accessory use, in which case the maximum height **is** 8 metres.
11. Locate the ARU within 50 metres of the main building.
12. Locate detached ARUs to maximize the amount of consolidated yard space that may be communal or divided into private amenity spaces.
13. Provide a walkway from the front door to the driveway or walkway connecting to the sidewalk.
14. Design ARUs as a small house, with distinct front doors and weather protection.



Maintain the residential nature of the property using similar materials, colours and proportions



Provide a walkway from the front door and outdoor amenity space



Design ARUs as a small house, with distinct doors and entrances

## 3.2.5 Low-Rise Residential Buildings

**Low-rise residential buildings** up to three (3) storeys are permitted across the Town's residential designated areas including the Community Living Area in Thornbury/Clarksburg and Residential/Recreational Areas. Taller mixed use buildings up to four (4) storeys, or fourteen (14) metres, may be permitted along Highway 26 in Thornbury, subject to the Town's Official Plan and **Section 4.2** of this document.

### Guidelines:

#### Orientation and Massing and Articulation

1. Design the building to be compatible with adjacent buildings and the surrounding neighbourhood.
2. Ensure the building length does not exceed 50 metres. Buildings longer in length than 50 metres should either be broken up physically or visually using step backs, colour, material variations and unique building articulation.
3. Orient buildings parallel to the street right-of-way to frame and animate the street.
4. Orient the front façade to face the public street and locate front doors to be visible and directly accessible to the public sidewalk.
5. Locate buildings at corner sites close to both street right-of-ways to reinforce the street edge. Ensure both facades are treated equal with high quality design and architectural detailing.
6. Align building setbacks with the established streetwall. Where appropriate, increased may be provided to accommodate pedestrian access or active outdoor use such as patios or amenity space.
7. Site buildings with appropriate setbacks to minimize the impacts, including shadowing and wind effects, on surrounding sites.



Design the building to be compatible with the character and style of the surrounding area



Orient the front facade to face the public street and locate front doors to be visible and directly accessible from the public sidewalk



Setback buildings to align with the established streetwall

8. Ensure new buildings do not obstruct views of Georgian Bay along streets that terminate at or close to the water's edge.
9. Provide additional design emphasis such as window treatment and architectural elements for buildings located at street intersections, gateways or terminating views along visual corridors through facade treatments, architectural elements and materials appropriate for prominent locations.
10. Locate primary building entrances to connect to a public sidewalk, with architectural features that emphasize its location.
11. Provide a combination of horizontal elements such as cornices and projections and vertical elements such as changes in material, building articulation, columns or other vertical design elements to create interest.
12. Ensure that the range of materials and colours used in building design achieves a unified image for the building and site.
13. Use durable, high-quality and locally sourced materials wherever feasible.
14. Design upper floor elevations with an articulated elevation, including different colours and materials from the base floor, a variety of windows, balconies and projections and recessions.
15. Design rooftop mechanical equipment as an integral part of the building design, including setting back equipment from the roof's edge or screening equipment.
16. Consider providing ground floor units with individual at-grade access to increase building pedestrian orientation while keeping in mind safety and security.
17. Include private balconies on upper levels of a building, with designs that account for sunlight, views of natural areas and landmarks.



Clearly define building entrances with special architectural features and glazing





Provide a combination of horizontal and vertical architectural elements to create visual interest



Ensure the range of materials and colours achieves a unified image for the building and site

## Site Design and Parking

18. Use a combination of street trees, foundation planting and decorative fencing within the site's landscape edges that provides a suitable visual edge while ensuring visibility into and from the site.
19. Frame the site and building entrance using landscaping treatments or streetscape furniture.
20.  Locate bicycle racks near building entrances to support convenient access and visibility.
21. Ensure waste storage areas are screened from view, while still easily accessible to ensure ease of use.
22. Wherever possible, locate parking at the rear of the building, not between building and public street right-of-way.
23. Locate parking areas close to the building entrance and provide an easily identifiable pathway to the entrance. Provide lay-by parking to support accessible pick-up, drop-off, and delivery functions for people of all ages and abilities.
24. On larger sites where there is more than one building, parking may be located internally between the buildings behind the front wall facing the street.
25. Where parking is located in the front yard, limit parking areas to a single or double loaded row, with a landscaped strip, fence, and/or wall between the street and parking.
26. Ensure pedestrian routes through sites and parking areas are safe, convenient and clearly demarcated. Ensure they are a similar size to a public sidewalk, are barrier-free and are served by adjacent shade trees and pedestrian lighting.
27. Divide larger surface parking areas into smaller parking areas through the use of use landscaped islands to minimize the visual extent of the paved area.
28. Locate snow storage areas to minimize impacts on streetscapes and pedestrian circulation. Snow storage should not obstruct sidewalks or required parking.
29.  Provide designated spaces for car share, carpool vehicles and Electric Vehicle (EV) charging, where feasible.








On larger sites with more than one building, locate parking internally between the buildings and ensure pedestrian routes are safe, convenient and clearly marked

### 3.2.6 Resilient and Energy Efficient Building Design

Resilient and energy efficient building design is a key priority and should be considered as part of the design and construction of new residential development. For all types of residential built form, the Town will encourage high-quality design and construction practices that reduce energy consumption, improve building performance, support year-round comfort for residents and respond to the local climate and character of The Blue Mountains.

#### Guidelines:

-  Design buildings to support passive solar gain, natural daylighting and natural ventilation, while limiting heat loss in winter and excessive heat gain in summer.
-  Orient and mass buildings to respond to sun exposure, prevailing winds and seasonal weather conditions, while maintaining a consistent relationship to the street and surrounding built form.
-  Encourage the use of enhanced insulation, high-performance windows and doors, air sealing, heat pumps and other energy efficient building systems, including shared or district heating systems where feasible, to improve building performance and reduce energy demand.
-  Design window placement, size and glazing to balance daylight access, energy performance, privacy and compatibility with the surrounding streetscape.
-  Incorporate architectural elements such as roof overhangs, canopies, porches, balconies, awnings, recessed entrances and covered walkways to provide shade, weather protection and year-round comfort.










Large windows, roof overhangs and building orientation help support passive solar gain, natural daylighting and seasonal shading while contributing to year-round residential comfort.



Roof overhangs and recessed upper-storey glazing help provide shade, weather protection and passive solar control while complementing architectural character.



Heat pumps support energy-efficient heating and cooling when integrated into the building design

6.  Design roof forms to accommodate resilient and energy efficient features such as enhanced insulation, solar-ready infrastructure, solar panels, snow management and durable roofing materials, where appropriate.
7.  Encourage the use of green roofs, white roofs or other high-reflectance and energy efficient roof treatments, where appropriate, to reduce heat absorption, improve building performance, support stormwater management and contribute to year-round comfort.
8.  Encourage the use of durable, low-maintenance and low-carbon building materials that are suited to local snow, wind, moisture and freeze-thaw conditions, reduce lifecycle environmental impacts and complement the character of the surrounding area.
9.  Integrate energy efficient mechanical systems, vents, utility meters and equipment into the building design to minimize their visual impact.
10.  Incorporate electric vehicle (EV) charging infrastructure, including EV-ready parking spaces and electrical capacity to accommodate future charging needs, where feasible.
11.  For multi-dwelling buildings, coordinate articulation, shared walls, rooflines and entrances to improve energy performance.
12.  Integrate resilient and energy efficient design features in a manner that complements the building's scale, massing, rooflines, materials and architectural character.



Design roof forms to accommodate solar-ready infrastructure or solar panels, as well as durable roofing where possible



Green roofs can reduce heat absorption, support stormwater management and improve building performance while contributing to year-round comfort and architectural character.



Durable, low-maintenance materials help buildings withstand local snow, wind and freeze-thaw conditions while supporting long-term sustainability.

# 4

## Downtown and Commercial Areas

The Blue Mountains is characterized by a diverse, reliant and innovative local economy. The Town comprises several downtown and commercial areas, each with their own vision and character.

Downtown Thornbury and Downtown Clarksburg are focal points for commerce in the Town and will continue to evolve over time to accommodate a mix of uses and variety of public spaces. The Highway 26 Corridor in Thornbury also accommodates a mix of commercial and service uses and has been identified as a priority area for mixed use and commercial development and revitalization. Craigleith Village, located at the eastern edge of the Town along Highway 26, is also prioritized to redevelop into a compact, mixed use community.

Development is anticipated to take place in a variety of settings, which may include stand-alone commercial buildings or mixed use buildings which accommodate a residential component. There are numerous opportunities throughout the Town including development on vacant properties, redevelopment of underutilized properties, additions to existing buildings and adaptive reuse of existing vacant buildings. Mixed use and commercial development in the Town will be both visually interesting and appropriately scaled while providing for a strong street edge presence and pedestrian-scaled facades.

This section of the Design Guidelines provides direction on streetscapes, site design and built form.

## 4.1 Downtown Streetscapes

This section of the Design Guidelines provides direction for streetscape elements in downtown areas, particularly applying to Thornbury and Clarksburg, as well as Highway 26 through Thornbury. As Craigleith Village develops, or any other downtown area, these Guidelines should also be referenced to ensure high-quality design of the streetscape. The Design Guidelines apply to the elements within the public right-of-way, from the curb to the building face.

Streetscape elements form an important part of the open space system and include components such as sidewalks, street trees and planting, street furniture, lighting and utility placement. Streetscape elements will vary based on the character of Downtown Thornbury and Downtown Clarksburg, as well as the envisioned character of Craigleith Village.



When streetscape elements are appropriately coordinated based on local context, they help to create an attractive, cohesive and safe environment for all users. Streetscapes should specifically be designed to support pedestrian-friendly spaces, active transportation and public safety. Design decisions should reflect current traffic realities and should be implemented incrementally over time. Streetscape design will principally be achieved by the owner of land adjoining the public realm and by works undertaken by the Town.

At a minimum, **6 metres should be provided between the curb and the building face**, including the Town-owned portion of the right-of-way and the building setback. Streetscapes should include a minimum **2 metre wide sidewalk** for pedestrians and a **1.5 metre boulevard** to accommodate landscaping and all street furniture.



## 4.1.1 Sidewalks

### Guidelines:

1. Construct sidewalks to municipal standards. The width of the hardscaped area or pedestrian walkway should respond to the land use context and accessibility requirements.
2. Ensure all streetscape elements are clear of the sidewalk, including trees, street furniture, utilities, bicycle parking and parking meters.
3.  Ensure sidewalks are direct, continuous, and generally located on both sides of all streets to support walkability and prioritize pedestrian safety.
4.  Design sidewalks and walkways to connect to other public realm components such as parks and open spaces and tie directly with trails and multi-use paths.
5. Eliminate or minimize grade changes at the street level to allow pedestrians to move directly from the street into buildings.



Ensure all sidewalks are clear of streetscape elements and provide direct paths of travel for pedestrians



The width of the hardscaped area or pedestrian walkway should respond to the land use context and accessibility requirements.

## 4.1.2 Transition Areas and Patios

### Guidelines:

1. Transition areas between the building and sidewalk may contain outdoor seating areas, patios, planters, signage, temporary retail displays and other elements that extend active uses outdoors and create visual interest to the streetscape.
2. Ensure the placement of street furniture and other elements in the transition zone does not obstruct the pedestrian clearway zone or pedestrian movement along the sidewalk.
3. Locate patios so that at least 2 metres of unobstructed route on the sidewalk is maintained.
4. Maintain at least 1 metre of direct and unobstructed route through a patio to the primary entrance of a business.
5. Ensure the alignment of the public sidewalk remains straight within the right-of-way, or alternatively, angled following the configuration of the bump-out along the street.
6. Design any patio structures, such as railings or walls, to complement the building's design using materials that allow visibility to and from the space.
7. For patios entirely on private property, use surface materials that complement those in the public right-of-way, although distinctive enough to define the boundary.



Design any patio structures, such as railings or walls, to compliment the building's design



Transition areas may contain outdoor seating areas, planters or temporary retail displays



Ensure the alignment of the public sidewalk remains straight within the right-of-way, or alternatively, angled following the configuration of the bump-out along the street

## 4.1.3 Street Trees and Plantings

### Guidelines:

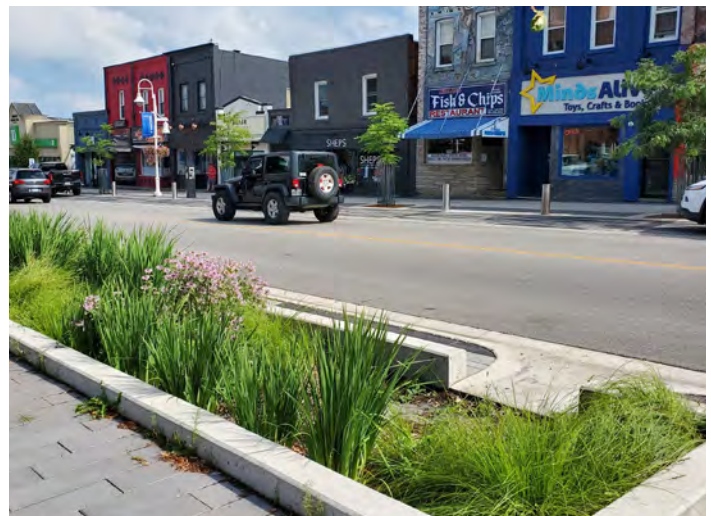
1. Select street tree species and plantings in accordance with the Town's Engineering Standards.
2. Choose tree species that create a tree canopy at its ultimate height which provides unobstructed views to the storefronts and signage of buildings.
3. Choose tree, shrub and other planting species that are native, non-invasive, low maintenance, salt tolerant and suited to the soil conditions in order to ensure they thrive in a downtown environment.
4. Plant trees along all public streets in a consistent pattern, preferably one tree per lot and coordinate with the location of street furniture and utilities.
5. Provide minimum 30 cubic metre soil volume for street trees to enable healthy and mature tree canopies and encourage continuous shared soil beds.
6. Provide seasonal interest for planters and boulevards through a combination of coniferous and deciduous plant species.
7. Coordinate tree and street light locations with above and below-grade utilities. Incorporate electrical receptacles, where appropriate, at all new tree installations for seasonal lighting opportunities in the downtown areas.
8. Prioritize shade tree species along sidewalks, pedestrian routes and gathering areas to provide shade, reduce heat impacts and support a comfortable pedestrian environment.



Provide seasonal interest through a combination of coniferous and deciduous plant species.



Plant street trees along all public streets in a consistent pattern



Choose planting species that are native, non-invasive, low maintenance, salt tolerant and suited to the soil conditions

## 4.1.4 Street Furniture

### Guidelines:

1. Choose and install street furniture of a consistent style and material in order to read as a single, coordinated entity, including: benches, trash and recycling receptacles, bicycle racks, tree guards and grates, banners and banner standards, hanging basket standards, and planters.
2. Ensure the style of street furniture is consistent with the desired downtown main street environment of Thornbury and Clarksburg.
3. Cluster or group street furniture wherever possible to minimize clutter in the streetscape and provide comfortable pedestrian refuge areas.
4. Locate street furniture in a manner that does not obstruct pedestrian circulation on the sidewalk and that is offset appropriately from on-street parking spaces.

### Benches

5. Install easily recognizable benches close to the entrances of designated heritage and public buildings, trails and walkways, bicycle racks, open space areas, parks and public art.
6. Orient benches to face the roadway or toward the sidewalk.

### Waste Receptacles

7. Install multi-purpose waste and recycling containers when additional waste containers are required or existing units are replaced.
8. Locate waste receptacles in proximity to seating areas, trails and walkways and park entrances.



Street furniture cluster in Downtown Thornbury






Cluster street furniture and public art in the streetscape



Orient benches to face the roadway or sidewalk


## Bike Racks

- 9.  Provide bike racks in frequent, visible and convenient locations including near building entrances, open spaces, trail connections and key destinations.
- 10.  Provide bike racks at a minimum rate of one per block and in proximity to all public buildings and open spaces.
- 11.  Introduce bike repair stations in high-use areas and gathering spaces.
- 12. Ensure bike racks are installed as close to, without being directly in front of, the main entrance(s) of a building or site.
- 13. Ensure bike racks have adequate clearance from driveways, curb ramps, transit loading areas and immediately adjacent to shelters, and utility poles.



Ensure bike racks are installed along the streetscape in close proximity to building entrances

## Transit Ready

- 14.  Design streetscapes to be transit-ready by incorporating flexible curbside space, pedestrian amenities and public realm elements that can support future transit service, including transit shelters, seating, lighting, wayfinding, accessible passenger waiting areas and safe connections to sidewalks, cycling facilities and nearby destinations.



Street furniture cluster in Blue Mountain Village

## 4.1.5 Street Lighting

### Guidelines:

1. Ensure lighting consists of a coordinated family of luminaires and poles with regard to design, materials and color.
2. Utilize the current street light standard in Downtown Thornbury to ensure visual consistency and continuity.
3. Design lighting to define and reinforce the hierarchy of street systems to promote a sense of site orientation and organization.
4. Provide illumination levels and lighting sources that minimize areas or points of glare while providing adequate levels of light for safety and security.
5. Affix additional amenities such as banner signage or hanging flower pots to light standards or hydro poles where minimum heights can be accommodated in order to add further visual interest and character.
6. Consider additional pedestrian scale lighting such as bollards or accent lighting within gateway areas or in areas to accent signage, murals or public art.
7. Locate light standards in a coordinated manner that does not obstruct pedestrian circulation on the sidewalk or driveways.



Light standard in Downtown Thornbury



Design lighting to foster a sense of place and orientation

## 4.1.6 Wayfinding and Directional Signage

### Guidelines:

1. Ensure a coordinated and comprehensive system of wayfinding signage is designed and oriented to balance needs of people traveling by foot, bicycle, transit or car.
2. Use simple and universally readable signs with a consistent design template which complements the design of the wayfinding signage.
3. Include, at a minimum, the following destinations in a comprehensive wayfinding program: civic or public buildings, public parking areas, open spaces and trails, and other significant destinations or features.
4. Ensure signage is visible and legible from the road right-of-way, be designed with high contrast, and comply with the Accessibility for Ontarians with Disabilities Guidelines.
5. Ensure the physical placement, installation and illumination of signs is suitable for all users of all abilities.
6. Construct signage from durable, high-quality material.
7. Ensure signs on heritage buildings are compatible in terms of heritage character, colour, and material, and should not obscure architectural details.




Develop a coordinated and comprehensive system of wayfinding signage



Use simple and universally readable signs with consistent design

## 4.1.7 On-Street Parking

### Guidelines:

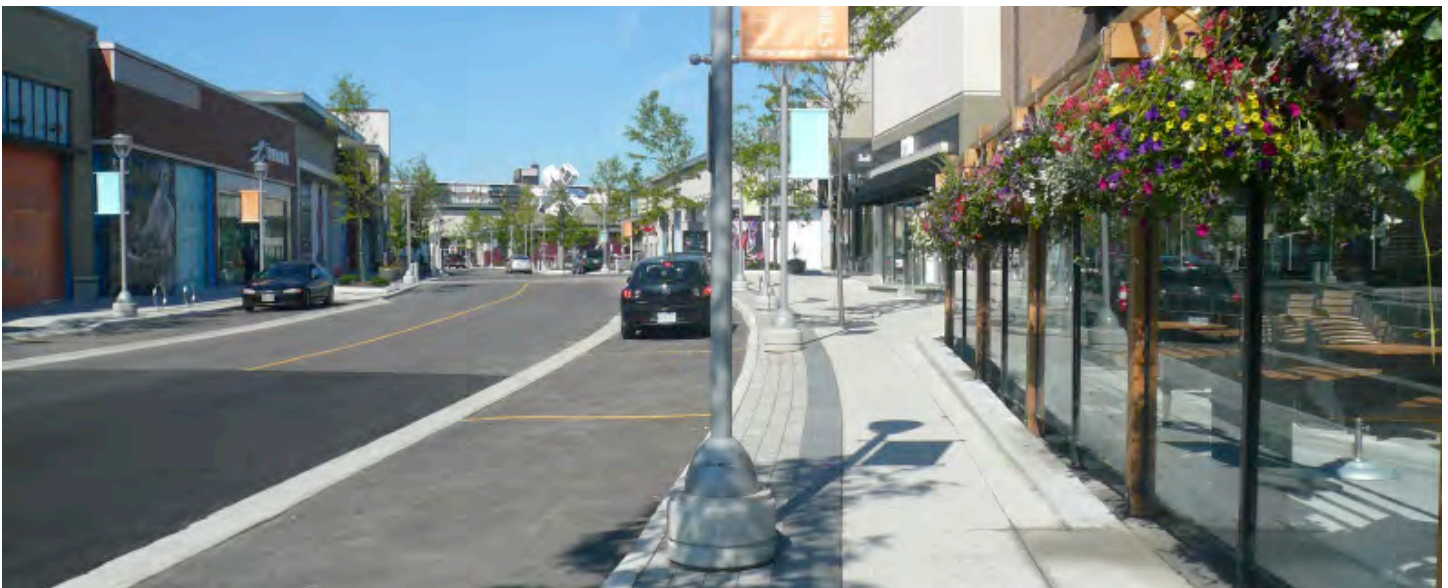
1. Provide on-street parking spaces on all streets, where feasible, where there are active uses fronting the street.
2. Consider on-street parking spaces for specialty vehicles, coordinated with accessible ramps to the sidewalk.
3. Ensure on-street parking spaces are coordinated with any curb extensions that may be incorporated at intersections. The preferred treatment for on-street parking is a parking bay which includes a curb-extension (bump-out) at the beginning and end.
4.  Consider permeable paving for on-street parking where appropriate as a low impact development measure for stormwater management and to visually enhance the street edge.
5. In Downtown Thornbury and Clarksburg, the temporary use of on-street parking areas for restaurant patios or bicycle parking is encouraged, subject to approval by the Town.



Street parking using a curb extension at the beginning and end



Consider permeable paving as Low Impact Development



Ensure on-street parking spaces are coordinated with any curb extensions and clearly marked

## 4.2 Built Form

The quality and design of mixed use and commercial development defines the character of the Town's several downtown areas and linear commercial streetscapes.

Both ***mixed use buildings*** and ***stand-alone commercial buildings*** are permitted in Downtown Thornbury (Bruce Street and Highway 26), Downtown Clarksburg and within the evolving Craigeleith Village area. Mixed use buildings are strongly encouraged on main streets, including along Highway 26 within Thornbury and Craigeleith Village to add to the overall commercial vibrancy of these corridors while providing for a range of housing options. The rehabilitation of commercial, residential or mixed use building spaces into housing units on upper storeys or in the rear floor space of a building is strongly encouraged.

Mixed use and commercial development in Downtown Thornbury along Bruce Street and all of Downtown Clarksburg is permitted up to three (3) storeys to respect existing low-rise, historic character.

Outside of the Thornbury Downtown Core along Bruce Street, compatible built form up to four (4) storeys is permitted, generally along Highway 26. Taller buildings should be located on appropriately sized and situated lots. A transition in height is required from the low-rise Thornbury Downtown Core along Bruce Street to any taller buildings along Highway 26.

This section of the Guidelines provides direction on built form and architectural elements including building scale, orientation and siting, character, entrances and openings.



## 4.2.1 Scale, Orientation and Siting

### Guidelines:

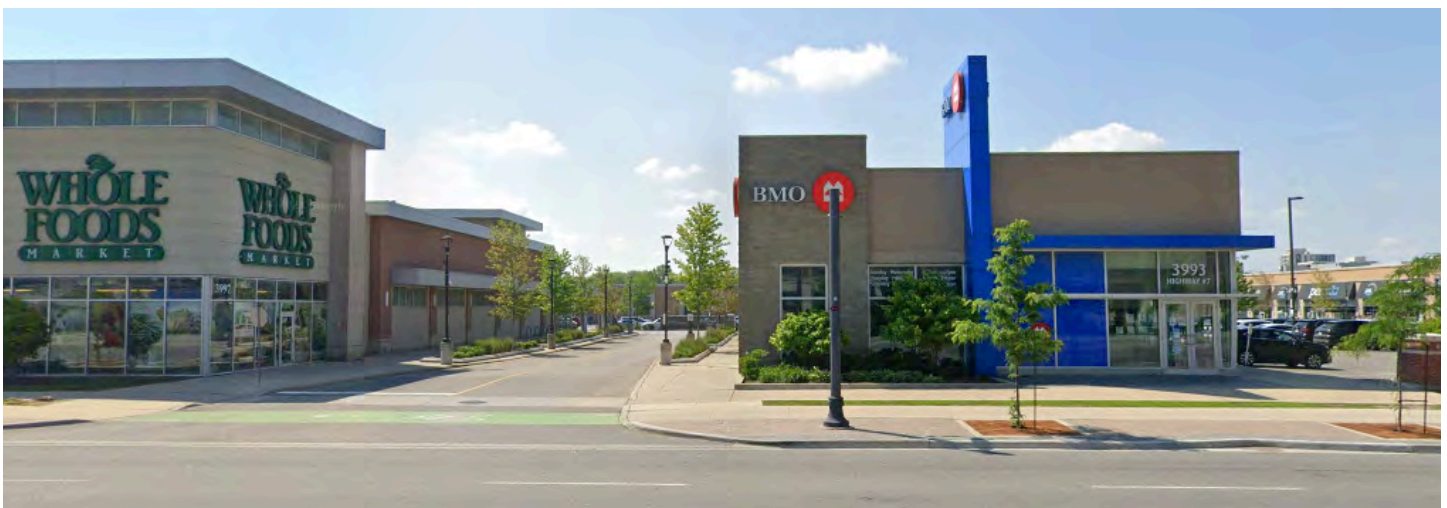
1. Orient all buildings parallel to the street right-of-way so that they frame and animate the street and strengthen the street edge's definition.
2. Where possible, design buildings with a minimum height of two (2) storeys.
3. For corner buildings located at street intersections, locate the tallest portion of the building along the street frontage to punctuate the street corner.
4. Set back new buildings and changes to existing buildings to align with the established streetwall.
5. Use larger setbacks for increased pedestrian access or active outdoor use, such as patio space accessible from the inside of the building.
6. Ensure the building length does not exceed 50 metres. Buildings longer in length than 50 metres should either be broken up physically or visually using step backs, colour, material variations and unique building articulation.
7. Ensure new buildings do not obstruct views of Georgian Bay along streets that terminate at or close to the water's edge.



Orient buildings to frame the street and provide additional height or architectural details for corners



Use larger building setbacks for active outdoor uses



Orient all buildings parallel to the street right-of-way so that they frame and animate the street and strengthen the street edge

## Mixed Use Buildings

8. Provide a minimum 4.5 metre floor to ceiling height for the first floor.
9. Front commercial uses along main streets, with residential uses and entrances fronting any side streets.

## Downtown Thornbury and Clarksburg

10. Set back new buildings and changes to existing buildings to align with the established streetwall.
11. Limit the height of new and renovated buildings to a maximum of three (3) storeys and ensure development respects the existing scale and massing of adjacent buildings.

## Highway 26 Through Thornbury

12. Generally setback buildings a minimum of 12 metres and a maximum of 16 metres from the front property line to maintain and enhance the open space character and feeling of spaciousness along Highway 26. Utilize building setbacks to provide additional open space that includes amenities including trees, seating, pedestrian scale lighting and public art to enhance the pedestrian experience along the street.
13. For buildings taller than three (3) storeys, provide an appropriate setback and transition in height adjacent to low-rise neighbourhoods such as step backs, stepping down or incorporating variation in building form.
14. For buildings greater than four (4) storeys, incorporate a minimum step back of 1.5 metres above the second or third storey, depending on existing streetwall and building height.
15. Design new buildings to respect a 45 degree angular plane measured from the property line which separates the lot from an adjacent lot with a low rise residential dwelling.



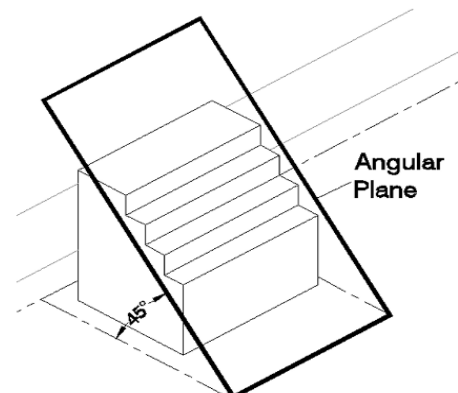
Provide a minimum 4.5 metre floor to ceiling height and wrap active commercial uses along the main facades



In Downtown Thornbury/Clarksburg, set back new buildings to align with the established streetwall and maintain a maximum three storey height



Design taller buildings to incorporate step backs above the second, third or fourth storey



Design buildings to respect a 45 degree angular plane

## 4.2.2 Building Design

### Guidelines:

1. Orient the front façade to face the public street and locate front doors to be visible and directly accessible to the public sidewalk.
2. Provide a combination of horizontal elements such as sign bands, cornices, and projections and vertical elements such as changes in material, building articulation, columns or other vertical design elements to create visual interest.
3. Design buildings to include variation such as slight projections, setbacks and recessed doors between different store fronts to easily identify the different retailers.
4. Ensure that the range of materials and colours used in building design achieves a unified image for the development. Use durable, high-quality and locally sourced materials wherever feasible.
5. Design multi-tenanted commercial buildings with a variety of colours, signage and materials, as well as articulation, windows and vertical delineation on the elevation so that individual units are differentiated.
6. Provide additional design emphasis for buildings located at street intersections, gateways or terminating views along visual corridors through facade treatments, architectural elements and materials appropriate for prominent locations.
7. Integrate building canopies or awnings into the building design to provide shelter from the sun or natural elements.



Design multi-tenanted buildings with a variety of colours, signage, materials, articulation and windows



Provide a combination of horizontal elements such as sign bands, cornices, and projections and vertical elements such as changes in material, building articulation and columns



Integrate canopies or awnings into overall building design

8. For corner buildings ensure both facades are treated equal. Where buildings are angled with the main entrance located at the corner, both façades are to be treated with equal high quality design and large storefront windows.
9. Wrap the primary façade storefronts around any corners to include a minimum of 6 meters of the secondary façade that matches the primary facade.
10. For commercial buildings, false upper building floors should not be created; however, they may be feasible in instances where the design/engineering of the building allows for upper floors to be inserted in the future.
11. Design rooflines with alternatives to a flat roof, such as pitched roofs or a combination of different types to promote visual interest.
12. Locate and design wall-mounted signs to complement the character and scale of the development, keeping in mind a balance between vehicular orientation and pedestrian orientation.
13. Incorporate high quality lighting and signage to support retail visibility, interaction and safety. Well-lit storefronts contribute to the character and activity of the street and provide a sense of identity.
14. Integrate rooftop mechanical equipment with the building design. Screen rooftop units and vents using materials that are complementary to the building.



Use a variety of rooflines to create visual interest



Design and locate signs to compliment the character of development



Ensure the facade of corner buildings are treated equally and match the primary facade, wrapping storefronts around any corners

## Downtown Thornbury and Clarksburg

15. Design new buildings to reflect the historic character and theme of the respective downtown area.
16. Building materials, such as steel and vinyl siding, which are not in keeping with the architectural character of the downtowns will be discouraged.
17. For infill development or additions to buildings within the existing core areas align architectural elements with those of the adjacent building to ensure visual continuity.
18. Traditional signage and lighting is preferred rather than florescent sign boxes and corporate signage.
19. For existing buildings in the Thornbury Downtown Core, improve any building façades that are wider than 15 metres by dividing the larger façade through a symmetrical pattern of bays to emphasize the individual units of the building or divide a large single store's façade.
20. Preserve and enhance the cultural and historic features that exist, which may include requirements for the restoration and enhancement of building facades
21. Restore any original architectural details and features on buildings.
22. Where an existing building lacks significant architectural detail or a new building is to be built on a vacant lot, the façade should be representative of, or consistent with, adjacent architectural styles



Design building elements to reflect the character of downtown areas



Divide larger facades through a symmetrical pattern of bays to emphasize individual units and single store facades



Choose materials and architectural elements that align with those of the original building or adjacent buildings

## 4.2.3 Entrances and Openings

### Guidelines:

1. Ensure the transition from the sidewalk to the retail or service space is seamless and accessible to all.
2. Ensure main entrances are grade-related, accessible and barrier-free.
3. Accentuate all main entrances by integrating intuitive signage, storefront window treatments, effective architectural features and hard and soft landscaping elements.
4. Provide expansive storefront windows for views to activities inside where possible, creating interest for pedestrians along the street.
5. Use clear windows and doors to make the pedestrian level façade of walls facing the street highly transparent. Ensure glazing occupies a minimum of 60% of the first storey façade.
6. Locate secondary entrances to connect directly to any the parking area.
7. Provide semi-private amenity spaces along the street front including patios, plazas, spill out retail, informal seating and shade trees to animate the street and encourage pedestrian activity.
8. Include overhead architectural features over entrances and openings that provide shade and reduce daytime heat gain (awnings, canopies, trellises, or cornice treatments), especially on south-facing facades.
9. Use upper storey windows to establish a consistent rhythm and visual interest to the streetscape.



Provide expansive storefront windows that wrap around any building corners



Accentuate entrances by providing overhead architectural features and signage



Use upper storey windows and building design features to establish a consistent rhythm along the street

## Downtown Thornbury and Clarksburg

10. Ensure that storefront elements are designed to perform the traditional functions of a storefront including providing area for display storage, allowing natural light into shops and allowing visual communication between the interior of shops and the pedestrian sidewalk.
11. Incorporate broad window treatments into the facade at street level to maintain an open, pedestrian friendly environment



Design storefronts to provide display storage areas and allow for natural light into shops and views into and out of the building

## Mixed Use Buildings

12. Clearly distinguish residential entrances from commercial entrances through building design and location through features such as canopies/overhangs, door setbacks and building articulation.
13. Where provided, upper storey residential apartment units or offices should be accessed via a consolidated lobby.
14. Ground floor residential apartment units (permitted along side streets) may either be accessed via the consolidated lobby or may incorporate individual unit entrances.



Provide upper storey residential unit access via a distinct separate entrance









Distinguish residential entrances through building design and features such as overhangs and setbacks

## 4.3 Site Design

This section of the Design Guidelines applies to site design for both commercial and mixed use buildings. Direction is provided for landscaping, pedestrian circulation, parking, utilities, servicing and lighting.

### 4.3.1 Landscaping

#### Guidelines:

-  Use a combination of street trees, foundation planting and decorative fencing within the site's landscape edges to provide an attractive edge while ensuring visibility into and from the site.
-  Plant native, salt and drought tolerant tree and shrub species.
-  Provide a minimum of 30m<sup>3</sup> of soil volume for each tree to enable healthy growth. Structural soil, root barriers, tree guards and tree grates are encouraged for trees that are planted near sidewalks and walkways.
-  Provide seasonal interest through the use of coniferous and deciduous plant materials throughout the site. Use deciduous trees on the south side of buildings to provide summer shade and allow passive solar heating in the summer. Coniferous trees on east and west facades provide protection from low-level sun glare.
-  Integrate low impact development features such as permeable paving, bioswales and rain gardens to support on-site stormwater management.
-  Use tree-pits and planter boxes as part of the storm water management strategy. Allow runoff from buildings to drain to these areas to be infiltrated/consumed by plantings.
7. Where commercial buildings abut residential buildings provide a landscape buffer.



Use landscaping and street furniture to provide an attractive edge along the street



Provide raised planters and planter beds along pedestrian routes






Plant a variety of native tree and shrub species



Utilize plantings as a landscape buffer

## 4.3.2 Circulation and Parking


### Guidelines:


1. Provide an internal pedestrian circulation system that is clearly defined, logical, and connected to the public sidewalk, building entrances and parking areas.
2. Differentiate pedestrian routes on vehicular driving surfaces with special treatment, such as different paving materials and raised walkways.
3. Locate parking at the rear of the building not between building and public street right-of-way.
4. Limit parking areas in the front yard to a single or double loaded row, with a landscaped strip, fence, and/or wall between the street and parking.
5. Coordinate access and parking between individual properties and provide access to parking the secondary street, whenever possible.
6.  Ensure pedestrian routes through surface parking areas are safe, convenient and clearly demarcated. Ensure they are a similar size to a public sidewalk, are barrier-free, and are served by adjacent shade trees and pedestrian lighting.
7. For longer blocks or properties, create central pedestrian connections that connect parking areas to building entrances. Design such connections with weather-protection and tree plantings, where feasible and where necessary.
8.  Divide larger surface parking areas into smaller parking areas through the use of use landscaped islands to minimize the visual extent of the paved area.
9.  Ensure parking islands are of sufficient size to accommodate the sustainable growth of planting materials and trees within the island area.



Provide parking to the rear of the building wherever possible and provide a well-connected pedestrian circulation system

10. Coordinate parking areas across several properties, or within one larger property, as much as possible particularly regarding access in order to limit the number of interruptions of the streetscape and public sidewalks.


 11. Where possible, provide shade trees around, and throughout parking areas to maximize shaded hard surfaces to reduce the heat island effect.


 12. Provide preferred parking spaces for carpool and car share vehicles, as well as Electric Vehicle (EV) charging.

13. Use landscaped buffers to physically and visually separate parking areas from the sidewalk or street.

14. Where parking in the front yard is unavoidable or not practical given the context of the surrounding area, screen such parking areas with fences/walls and the landscaping to minimize the visual impact on the streetscape.

15. Ensure that any screening methods for parking be well-maintained to avoid unsightly conditions that negatively impact the pedestrian safety and the area's character.

 16. For parking lots, aim to plant one tree planted per four parking spaces. Orient trees to provide maximum shade during summer conditions (i.e. on south side of spaces).

 17. Locate bicycle racks near building entrances to support convenient access and visibility.

18. Provide lay-by parking in proximity to building entrances to support accessible pick-up, drop-off, and delivery functions for people of all ages and abilities.



Provide shade trees around and throughout parking areas



Use landscaped buffers to physically separate parking areas



Screen parking areas using landscaping, open spaces and hardscaped elements

### 4.3.3 Utilities and Servicing

#### Guidelines:

1. Screen all utility equipment and utility meters from public view either by locating within buildings or visually screening it through site and building design.
2. Locate service areas, including areas for loading/unloading and garbage, in locations that are not directly visible from a public street, such as in the rear yard of a building or in a properly screened portion of the side yard.
3. Coordinate and share service areas between buildings or within developments as much possible to prevent disruptions to vehicular or pedestrian flows.
4. Ensure service areas are screened appropriately, through landscape materials, fencing or building design, from the views of adjacent properties or from the upper stories of the building to which they serve.
5. Design any screening structures so that they complement the character form, materials, and colours of the building.
6. Locate the accesses to service areas from secondary streets or the rear of buildings wherever possible to reduce the number of driveways.
7. Site all rooftop equipment, such as HVAC equipment or green energy infrastructure, so that they are setback from the roof edge and/or screened through roofline design elements wherever possible.
8. Include adequate space for waste management areas to accommodate collection containers for general waste, recyclables and organics.



Locate service and loading areas away from street view




Use fencing as a method to screen waste and recycling areas



Include adequate space for waste management areas

## 4.3.4 Site Lighting

### Guidelines:

1. Ensure that buildings and sites are not over-lit in order to maintain a desirable nighttime setting and environment.
2. When comprehensively planning lighting for a site, balance the need for safety and security with the reduction of energy consumption and nuisance impacts.
3. Light areas on the site used by pedestrians at night, including surface parking lots; building entrances; sidewalks and walkways; garbage disposal areas; and other areas.
4. Locate and direct building and sign lighting to light the intended area of illumination and limit off-site glare impacts on adjacent buildings or properties.
5. Incorporate lighting at regular intervals to prevent the creation of light and dark pockets to ensure visibility into and out from all areas on the site requiring lighting.
6. Design lighting poles and fixtures to be consistent with and complement the architecture of the building and the site.
7.  Ensure light fixtures and Dark Sky Nighttime Friendly compliant.
8. Use pedestrian-scaled lighting, such as low profile fixtures, along pedestrian routes through an off-street parking area.
9. Coordinate the location of lighting and lighting fixtures with pedestrian routes and plantings.



Design lighting to compliment the building design and provide an appropriate level of light for a desirable nighttime setting



Design lighting elements to be consistent with building architecture and provide a unique sense of place for areas



Incorporate lighting at regular intervals to ensure visibility

# 5

## Urban Employment Area

Thornbury and Clarksburg's Urban Employment Areas comprise a variety of employment, industrial and manufacturing sites at the eastern edge of the Settlement Area. Predominant access is provided from Highway 26 and Grey Road 2.

The overall goal for employment and industrial development in the Town is to ensure there will be variation in the types of businesses, as well as consistent application of design principles to provide attractive street edges and efficient site design that minimizes any potential compatibility impacts on surrounding areas.


This section of the Design Guidelines provides direction on built form design, as well as site design including landscaping, vehicular and pedestrian circulation, parking, loading and servicing areas, signage and lighting.



## 5.1 Built Form

A range of uses are permitted in the Urban Employment Area including manufacturing, assembly, processing and fabrication, as well as storage and warehousing and wholesaling establishments.

### Guidelines:

1. Use architectural features and materials to emphasize main building entrances, particularly those facing a public street.
2. Use architectural detailing to break up long wall elevations, including the use of windows, projections and recessions, and changes in building material or colour.
3. Design roof forms to be compatible with the style and massing of the building, and use roof materials and colours that complement the overall design.
4. Use changes in the building materials at wall projections or recessions.
5. Use high quality exterior cladding materials such as brick, stone, steel, glass and metal paneling, particularly on publicly facing elevations. Coordinate all materials, colours and finishes on all exterior elevations to achieve continuity.
6. Enclose or screen rooftop mechanical equipment from view of streets.
7. Ensure the architectural style of buildings accommodating multiple tenants is cohesive over the entire building.
8.  Design industrial buildings to support resilient and energy efficient performance through durable exterior materials, high-performance building envelopes, enhanced insulation, energy efficient windows and doors, efficient mechanical systems, and roof forms that can accommodate green roofs, solar panels or other energy efficiency measures, where feasible.



Use architectural detailing to break up long wall elevations



Use architectural features and materials to emphasize main building entrances





Coordinate all materials, colours and finishes to achieve overall building continuity

## 5.2 Site Design

### 5.2.1 General Site Design

#### Guidelines:



-  Incorporate any existing natural features such as existing trees, contours and water courses into site design, where feasible.
- Site buildings as close to the street edge as much as possible to frame the street and ensure parking and other industrial uses are located to the rear of the building.
- In accordance with the Town's standards, site buildings and landscaping to ensure that adjacent properties are protected from site illumination, noise, odour and outdoor service areas.
- Site buildings and locate windows to maximize informal surveillance opportunities by building users.
- Incorporate outdoor amenity areas into the overall site design, defined by building facades, fencing or landscaping. Locate outdoor amenity areas away from loading, storage or other noisy areas.
- Locate all parking areas and open spaces to maximize natural surveillance from buildings, public roads and walkways.
-  Incorporate low impact development measures within site design to support stormwater infiltration, runoff management and improved environmental performance, where feasible.



Site buildings close to the street edge and ensure main parking and industrial areas are located to the rear

## 5.2.2 Landscaping

### Guidelines:

1. Incorporate existing site features into the landscape design, where practical, taking advantage of on-site conditions such as view corridors or existing trees.
2.  Select native and non-invasive plant species with regard to their characteristics of soil type, sun, root spread, growth rate, density of canopy and salt tolerance.
3. Group plant materials to frame building elevations, add visual interest to blank building facades and accentuate building entrances.
4. Use landscape plantings and elements to assist in visually breaking up longer building wall elevations, coordinated with architectural elements and details on the wall elevation.
5. Use plantings and grading to define the boundaries of site, reduce the impacts of noise, provide a visual buffer between adjacent development and prioritize Crime Prevention Through Environmental Design (CPTED).
6.  Integrate low impact development features such as permeable paving, bioswales and rain gardens to support on-site stormwater management.



Group plantings to create entrance features and an attractive streetscape from public view



Group plant materials and furniture to frame building entrances



Plantings help to define the site and provide a visual barrier to prioritize Crime Prevention Through Environmental Design (CPTED)

7. Locate plant materials so that they do not interfere with sight lines at driveway intersections, lighting and emergency apparatus such as fire hydrants.
8. Plant parking islands with plant material that are salt and drought tolerant and easily maintained. Use hardy ground covers, mulch or similar materials, in parking lot landscape islands.
9. Ensure landscape strips along the outside edges of parking areas abutting public streets or adjacent properties are at least 3 metres wide, and planted with tree and shrubs.
10. Balance landscape screening objectives with the need with views into spaces and buildings so that users may be seen from different vantage points.
11. Use landscape materials to screen and buffer service areas on the site, such as waste disposal, loading areas, or open storage areas. This could consist of a wall or fence, a landscaped screen, dense landscaping planting, a landscaped berm, or a combination of these features.
12. Consider outdoor furniture and fixtures such as special lighting, trellises, arbours, raised planters, benches and fencing for outdoor amenity areas on the site.



Plant parking islands and walkways to main entrances with a variety of plant materials while also incorporating pedestrian scaled lighting, seating and bicycle racks

## 5.2.3 Circulation and Parking

### Guidelines:

1. Incorporate a well-defined and continuous pedestrian system on the site with connections to the public street, parking areas and outdoor amenity areas.
2. Ensure pedestrian connections are barrier-free and are provided directly from the public street sidewalk to the principal building entrance and parking areas.
3. For larger developments, incorporate major pedestrian routes that are easily identifiable through the use bollards, trees, continuous paving materials, signage and lighting.
4. Design walkways to be direct, follow natural desire lines and avoid unobstructed sight lines.
5. Minimize the number of driveway connections to the public street and consider common driveways to further minimize the number of driveways access points.



Incorporate a well-defined and continuous pedestrian system with access to the public street, parking and outdoor amenity areas



Incorporate pedestrian routes that provide access to all main building entrances and safe crossings beyond driveway and parking areas

6. Locate driveways to provide easy access for staff, visitors, delivery vehicles and emergency vehicles.
7. Locate driveways opposite existing or proposed driveways and streets to avoid offset intersections and traffic difficulties.
8. Locate driveways for corner lots away from the street intersection.
9. Provide sufficient area on the site for truck movements.
10. Use planted parking islands and planting areas to break up large parking lots, that are raised and at least 2.5 metres in width.
11. Align rows of parking perpendicular to the building for larger parking areas to minimize the number of crossings of drive aisles for pedestrians.
12. Locate well-drained snow storage areas adjacent to parking areas and away from catch basins, if possible, if snow will not be trucked off site.
13. Provide bicycle racks or indoor bicycle storage near entrances to buildings.
14. Provide designated spaces for car share, carpool vehicles, and EV charging in larger developments, where feasible.



Use planted parking islands to break up large parking lots

## 5.2.4 Loading and Service Areas

### Guidelines:

1. Orient loading bays and other service areas away from public street views, preferably screened from the street by building mass, fencing or screen walls compatible with the building architecture.
2. Locate waste storage areas inside buildings, wherever possible. Where necessary, locate outdoor storage in the rear and interior side yard of the building, although not in rear yards that face major roads.
3. Use building design, siting, landscaping and planting or fencing to screen views from the public street to outdoor waste storage areas.
4. Locate utilities underground, wherever possible, to improve the appearance of the development. Where above ground utilities are necessary, ensure their design is integrated and compatible with other site elements and screened from public view having regard for maintenance and access practices.



Orient loading bays and service areas to the side or rear of buildings

## 5.2.5 Signage

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### Guidelines:

1. Ensure signs complement the overall architecture and landscape design of the site.
2. Incorporate building identification signage as an integral, coordinated element of the principal building facade that is compatible with the building design, scale, colour and materials.
3. Coordinate thematic sign design for multiple tenant developments to contribute to a unified building presence.
4. Provide directional signage, as necessary, to assist in the orientation of pedestrians and traffic to the street, parking, service and open space areas.
5. Locate signs where they will not obstruct sight lines, driveways and intersections or interfere with pedestrian or motorist safety.
6. Select landscaping around the base of signs that ensures the continued visibility of signage as the landscaping matures.



Incorporate building signage as an integral component of site design

## 5.2.6 Lighting

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### Guidelines:

1. Design site lighting as an integrated system that considers all pedestrian, motorist and building needs.
2. Design lighting systems that carefully consider the areas to be lit, only illuminating areas which need to be illuminated given necessary activities and routes.
3. Avoid over-lighting a site, preferably using more fixtures with low wattage than few fixtures with higher wattage.
4. Focus lighting on pedestrian areas, clearly identifying pedestrian walkways and building entrances, at a general height of approximately 3.5 to 4 metres.
5. Integrate lighting fixtures and poles with the overall architecture and landscape design of the project.
6. Use lighting to accent and highlight building, signage and landscape features where appropriate.



Focus lighting on pedestrian areas



# Appendix A

## Community Design Checklist

This Community Design Checklist is intended to support the implementation the Community Design Guidelines through the development process. The checklist is organized into two components: **Volume 1 – Community Design Guidelines** and **Volume 2 – Community Vision and Structure**. Both sections of the checklist are to be completed for the proposed development, as applicable, based on the location, context, scale and nature of the proposal.

Refer to **Volume 1, Section 1.4.1: Implementation** for further details.

### Volume 1 Checklist

**Volume 1** contains the Community Design Guidelines. The applicable sections below should be completed for each proposed development, including an indication of whether the proposal achieves the intent of each guideline and any supporting comments, where appropriate.

Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
<b>Natural Environment</b> (Refer to Volume 1, Section 2)		
	1. Are existing natural features (woodlands, watercourses, significant trees, hedgerows, vegetation slopes and/or natural open spaces) integrated into the overall site, street and/or block design?	
	2. Are natural features, parks and/or open spaces framed by single-loaded streets or other approaches that support visibility, safety and access?	
	3. Are existing healthy trees proposed to be protected and retained by sensitive site and building design?	
	4. Are native, non-invasive and climate-resilient plantings proposed to support biodiversity, habitat function and natural character?	



Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
	5. Does the development provide sufficient landscaped open space to support stormwater infiltration, retention and runoff management?	
<b>Public Realm, Public Art and Cultural Heritage</b> (Refer to Volume 1, Section 2.6)		
	6. Does the proposal incorporate opportunities for public realm improvements, public art or cultural heritage integration that contribute to character, identity and visual interest?	
<b>Neighbourhood Design</b> (Refer to Volume 1, Section 3.1)		
	7. Are streets and blocks designed to support walkability, connectivity and access to parks, trails, open spaces and/or community amenities?	
	8. Are development blocks along Local Roads designed with a maximum length of 250 metres to support walkability, connectivity and pedestrian access.  Where development blocks exceed 250 metres along Collector Roads or Highway 26, are mid-block pedestrian connections or walkways provided to improve pedestrian access?	
	9. Does the proposed development incorporate a mix of lot sizes, housing forms and building types?	
	10. Does the development incorporate site and building design measures that support passive solar design, natural ventilation, tree preservation, street tree planting and/or low impact stormwater management?	
	11. If provided, are shared private outdoor amenity areas designed as visible, central, accessible and functional focal points for residents?	



Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
<p><b>Single Detached, Semi-Detached and Townhouse Dwellings</b></p>		
<p>(Refer to Volume 1, Section 3.2)</p>		
	<p>12. Is the dwelling designed with a primary façade and front door that face and is accessible directly to the public sidewalk?</p>	
	<p>13. Is the building designed to provide variation in elevation, articulation, materials, colour, rooflines, windows and architectural details while maintaining compatibility with its surroundings?</p>	
	<p>14. For corner buildings, are both street-facing façades designed with a consistent level of architectural quality, including coordinated materials and enhanced architectural treatment?</p>	
	<p>15. Does the design of the garage and/or driveway reduce visual prominence from the street?</p>	
	<p>16. Are rooflines, upper storeys, garages, driveways and parking areas designed to reduce visual impacts and provide appropriate transition to adjacent dwellings?</p>	
<p><b>Low-Rise Residential Buildings</b></p>		
<p>(Refer to Volume 1, Section 3.2)</p>		
	<p>17. Is the building designed to be compatible with adjacent buildings and its surroundings through appropriate height, massing, building setbacks, roof form and façade design?</p>	
	<p>18. Is the building designed to be less than 50 metres in length? Are longer buildings broken up physically or visually using stepbacks, colour, material variations and unique building articulation?</p>	
	<p>19. Does the building frame and animate the street, with the front façade and primary entrances oriented to the public realm?</p>	



Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
	20. Does the building provide appropriate setbacks, stepbacks and transition to minimize impacts on surrounding properties (shadow, privacy, wind, etc.)?	
	21. Are mechanical equipment, waste storage, utilities and servicing areas integrated or screened from public view?	
	22. Are bicycle racks, pedestrian routes, lay-by parking and drop-off areas located to support safe, convenient and accessible pedestrian movement?	
	23. Are parking areas located away from the street edge, where feasible, and designed with landscaping, shade trees, clear pedestrian routes, and lighting?	
	24. Are parking areas located away from the street edge, where feasible, and designed with landscaping, shade trees, clear pedestrian routes, and lighting?	
<b>Commercial and Mixed Use Development</b> (Refer to Volume 1, Section 4.2 and 4.3)		
	25. Is the building oriented to the street with the front façade, main entrances and active uses facing and directly relating to the public realm?	
	26. Is the building height, massing, scale and articulation appropriate for the surrounding context?	
	27. Are entrances, storefronts, windows and openings visible, accessible, grade-related and designed to support an active pedestrian environment?	
	28. Is landscaping used to enhance the streetscape, frame buildings, define site edges and provide visual buffering to parking areas where applicable?	
	29. Are pedestrian routes clearly defined, barrier-free and safely connected to building entrances, sidewalks, parking areas and outdoor amenity spaces?	



Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
	30. Are bicycle racks, pedestrian routes, lay-by parking and drop-off areas and loading/servicing areas located to support safe, convenient and accessible pedestrian movement?	
	31. Are parking areas designed to reduce visual impacts through location, landscaping, screening, shade trees, landscaped islands and pedestrian connections?	
	32. Does the site incorporate low impact development measures, sustainable landscaping practices and opportunities for stormwater infiltration where feasible?	
	33. Are servicing areas, waste storage, utilities and mechanical equipment integrated into the building design or appropriately screened from public view?	
	34. Is site lighting designed to support safety, accessibility and wayfinding while being Dark Sky Nighttime Friendly compliant?	
<b>Development in Downtown Areas</b> (Refer to Volume 1, Section 4.2)		
	35. Does the development reinforce the established character, scale, built form and streetscape pattern of the Downtown?	
	36. Does the building align with the established streetwall and contribute to a pedestrian-oriented public realm through storefront design, entrances, transparency and façade articulation?	
	37. Are building materials, signage, lighting and heritage features designed to compliment, preserve or enhance the architectural character of the Downtown?	



Complies; Partially Complies; Does Not Comply; or Not Applicable	Guideline	Description of How the Guideline Direction is Addressed <i>Provide document/plan/drawing reference(s) as applicable</i>
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### Development Along Highway 26 in Thornbury

(Refer to Volume 1, Section 4.2)

	38. Does the development maintain the open-space character and spaciousness associated with the Highway 26 Corridor?	
	39. Do building setbacks incorporate landscaping, pedestrian amenities, lighting, seating, public art or other streetscape improvements, where appropriate?	
	40. Are taller buildings appropriately stepped back and transitioned to adjacent low-rise residential areas, where applicable?	
	41. Does the development contribute positively to the visual character of the Highway 26 corridor through high-quality architecture and site design?	

### Employment Area Design

(Refer to Volume 1, Section 5)

	42. Does the building incorporate high-quality materials, architectural detailing and building articulation along public-facing elevations?	
	43. Are principal building entrances clearly visible and emphasized?	
	44. Is the site designed to incorporate landscaping, tree planting and low impact development measures, where feasible?	
	45. Are pedestrian routes direct, accessible and connected to entrances/parking?	
	46. Is bicycle parking provided in convenient and visible locations?	
	47. Are parking, loading, servicing and waste areas designed to minimize visual impacts through landscaping, building placement or screening?	
	48. Is site lighting designed to support safety, accessibility and wayfinding while being Dark Sky Nighttime Friendly compliant?	

# Volume 2 Checklist

**Volume 2** includes area-specific objectives to guide development and design within each community in the Town. Please select and complete the checklist that corresponds with the location of the proposed development and indicate whether each design objective has been met.

## 1. Thornbury and Clarksburg

### 1.1 Residential Neighbourhoods

Applies to residential and neighbourhood development in Thornbury and Clarksburg.

Yes; No; or Not Applicable	Objective
	1. Enhance the small-town character of existing and established residential neighbourhoods in Thornbury and Clarksburg by ensuring development and redevelopment is compatible with the scale and density of existing development.
	2. Encourage the development of a broad range of housing types, from single-detached and semi-detached dwellings to townhouses and low rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.
	3. Encourage gentle infill and intensification in neighbourhoods through well-designed additional residential units and converted dwellings.
	4. Create and maintain walkable neighbourhoods that are well connected to their surrounding downtown area, parks, open space and community amenities
	5. Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

### 1.2 Downtown Thornbury

Applies to development or redevelopment in Downtown Thornbury.

Yes; No; or Not Applicable	Objective
	1. Preserve the historic low-rise character of Thornbury's downtown core along Bruce Street.
	2. Ensure the scale and location of new development or redevelopment maintains and/or enhances the distinct heritage character of Downtown Thornbury.
	3. Reinforce the function of Downtown Thornbury as a primary cultural, business, entertainment and commercial focal point.
	4. Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.

Yes; No; or Not Applicable	Objective
	5. Encourage coordinated streetscape and façade improvements geared to the pedestrian that revitalize the cultural and historic character of Thornbury.
	6. Preserve and enhance cultural and historic features that exist within Downtown Thornbury, including the restoration and enhancement of building facades.
	7. Create new community focal points in Downtown Thornbury by encouraging the creation of new public spaces, pedestrian connections and public art.
	8. Enhance the pedestrian experience and connections along Bruce Street from the downtown core to Thornbury Harbour and the Georgian Bay waterfront.

### 1.3 Highway 26 Through Thornbury

Applies to development or redevelopment along Highway 26 in Thornbury.

Yes; No; or Not Applicable	Objective
	1. Establish Highway 26 through Thornbury as a priority area for mixed-use development, redevelopment and intensification.
	2. Ensure development fronting onto Highway 26 is held to a high standard to building, landscape and site design, as the main gateway to and from Thornbury.
	3. Encourage the development of a broad mix of uses, including residential, to provide access to range of amenities, as well as vibrant and safe public places within walking distance.
	4. Ensure the low-rise character of adjacent neighbourhoods and Downtown Thornbury is protected through compatible intensification and appropriate transition.
	5. Maintain and enhance the open space character of properties along Highway 26 and ensure the continued and improved feeling of spaciousness through development and redevelopment.

### 1.4 Downtown Clarksburg

Applies to development or redevelopment in Downtown Clarksburg.

Yes; No; or Not Applicable	Objective
	1. Reinforce the vibrancy of Downtown Clarksburg as a local, vibrant community with a strong sense of place tied to arts and culture.
	2. Ensure the scale and location of new development or redevelopment maintains or enhances the built form and streetscape character of Downtown Clarksburg.

Yes; No; or Not Applicable	Objective
	3. Reinforce the function of Downtown Clarksburg as a mixed-use focal point for the surrounding community, accommodating cultural, business, entertainment, commercial and service uses.
	4. Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.
	5. Encourage coordinated streetscape and façade improvements geared to the pedestrian that build on the sense of place for Clarksburg.
	6. Preserve and enhance cultural and historic features that exist within Downtown Clarksburg, including the restoration and enhancement of building facades.
	7. Create new community focal points in Downtown Clarksburg by encouraging the creation of new public spaces, pedestrian connections and public art.

## 1.5 Urban Employment Area

Applies to employment area development or industrial development.

Yes; No; or Not Applicable	Objective
	1. Prioritize opportunities for a diverse range of existing and new businesses to grow and develop in Thornbury in strategic locations.
	2. Ensure employment area development fronting onto the public realm is held to a high standard of building, landscape and site design.
	3. Ensure employment and industrial uses are adequately buffered and screened from any adjacent residential, open space or recreational use.

## 2. Craigleith

### 2.1 Residential Neighbourhoods

Applies to residential and neighbourhood development in Craigleith.

Yes; No; or Not Applicable	Objective
	1. Enhance the character of existing residential neighbourhoods in Craigleith by ensuring development and redevelopment is compatible with the scale and density of existing development.
	2. Encourage the development of a broad range of housing types and tenures, from single-detached and semi-detached dwellings to townhouses and low-rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.

Yes; No; or Not Applicable	Objective
	3. Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
	4. Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space and community amenities.
	5. Prioritize pedestrian-oriented design, cycling facilities and trail connections throughout Craigleith.
	6. Reinforce the uniqueness of each neighbourhood, with a focus on distinct heritage characteristics, civic and gathering spaces, natural features and areas, recreation amenities and accessibility.
	7. Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

## 2.2 Craigleith Village

Applies to new development in Craigleith Village.

Yes; No; or Not Applicable	Objective
	1. Develop a strong identity for Craigleith Village that accommodates a mix of residential, commercial and institutional uses and creates a new central node for residents and visitors.
	2. Ensure the scale and pattern of development in Craigleith Village respects surrounding residential lands and provides an appropriate transition.
	3. Encourage redevelopment in Craigleith Village to maximize residential intensification opportunities and variety through building sizes and inclusion of residential units above commercial uses.
	4. Encourage new coordinated streetscapes geared to the pedestrian, as well as new community focal points including public squares, that create a unique sense of place for Craigleith Village.
	5. Ensure the community road and trail network provide residents with a safe, functional and attractive neighbourhood that offers access to sustainable natural areas including wetlands, tree cover and the shoreline.
	6. Encourage the development of new recreation opportunities are tied to the environment and active, healthy living.

## 3. Lora Bay

Applies to residential and neighbourhood development in Lora Bay.

Yes; No; or Not Applicable	Objective
	1. Encourage development that is in keeping with the evolving open space and built form character of Lora Bay.
	2. Encourage a mix a broad range of housing types in appropriate locations while maintaining compatibility with existing adjacent character.
	3. Encourage gentle infill in neighbourhoods through well-designed additional residential units.
	4. Ensure neighbourhoods are well connected to parks, open space, the waterfront and community amenities.
	5. Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

## 4. Camperdown

Applies to residential and neighbourhood development in Camperdown.

Yes; No; or Not Applicable	Objective
	1. Ensure new residential development reinforces the uniqueness of each neighbourhood area in Camperdown, including the prominent lot pattern, scale, built form characteristics, landscape and open space character.
	2. Encourage a broad range of housing types while maintaining compatibility with existing character.
	3. Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
	4. Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space, the waterfront and community amenities.
	5. Prioritize pedestrian-oriented design, cycling facilities and trail connections.



## 4. Villages and Hamlets

Applies to development or redevelopment in the Town’s Villages and Hamlets.

Yes; No; or Not Applicable	Objective
	1. Ensure any new development or redevelopment is compatible with the character, role, scale and function of the surrounding natural, agricultural and rural landscape.
	2. Ensure Hamlets are maintained as focal points for the surrounding rural community and tourism with a mix of diverse and compatible uses in proximity to each other.
	3. Protect and enhance the natural character of the Niagara Escarpment as a unique and important feature of the Town.
	4. Carefully control new residential development in Hamlet Areas in order to maintain existing scale and character.
	5. Provide opportunities for small-scale commercial and tourism related uses that are compatible with the character and scale of these areas.
	6. Preserve and enhance the natural and historic features that exist in each rural community.



# Town of The Blue Mountains

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## Community Design Guidelines Volume 2 - Community Vision and Structure



June 2026

# Volume 2 - Vision and Structure

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# 1

# Community Design Vision and Guiding Principles

## 1.1 Community Design Vision

The Community Design Guidelines are informed by the Community Vision, Guiding Principles, Goals and Strategic Objectives of the Town's Official Plan. Both the Official Plan and Design Guidelines are based on the collective vision for a **healthy, complete community** that **protects natural and rural resources, supports sustainability, values heritage** and **celebrates community character**.

The Town of The Blue Mountains is committed to **high-quality design** of the public and private realm across the Town. The Design Guidelines will ensure development and redevelopment enhances the **unique character** of the Town's various communities, complements the Town's **natural and cultural heritage** and contributes to the **overall vibrancy** of The Blue Mountains.

## 1.2 Community Design Principles

To implement the Town's vision, the following Community Design Principles will be used to guide development, redevelopment and design across The Blue Mountains:



**Sustainable Design**



**Natural Character**



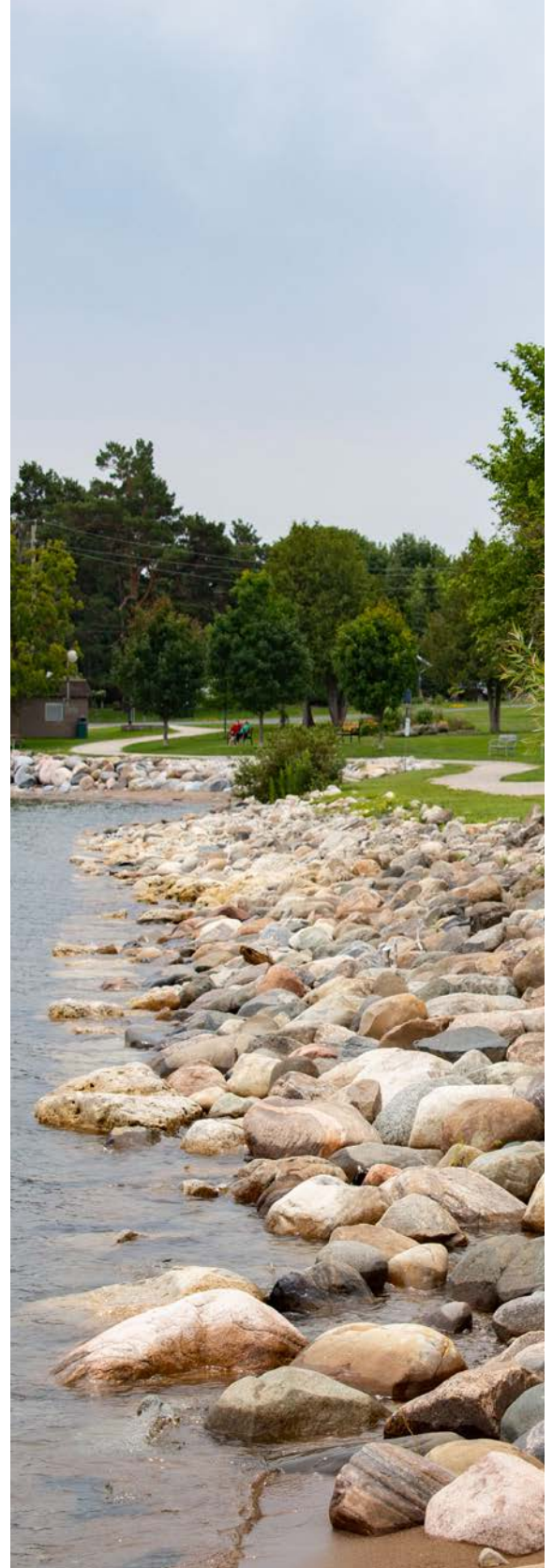
**Community Character**



**Tourism and Recreation**



**Cultural Heritage**





## Community Character

### Design Principles:

- Ensure development is designed to reinforce the Town's various communities as diverse, livable, safe, thriving and attractive.
- Foster a strong sense of place through high quality community design that considers both the public and private realm and interconnected open space network.
- Reinforce the uniqueness of each community across the Town, focusing on distinct heritage characteristics, prominent natural features, public art, cultural facilities, civic and gathering spaces, human-centered design and accessibility.
- Encourage the continued revitalization of Downtown Thornbury and Downtown Clarksburg through redevelopment and public realm improvements that reflect cultural and historic character and promote a mix of community uses and attractions that reinforce their function.
- Encourage investment and development in Craighleith Village that advances its function as a new vibrant, mixed use focal point for the community.
- Enhance gateways into community areas and revitalize the Highway 26 Corridor by recognizing it as a significant transportation and scenic corridor through the Town with views and vistas of Georgian Bay and the Niagara Escarpment.
- Retain the character of existing and well-established neighbourhoods while supporting infill, intensification and redevelopment that fits into and respects the scale and built form of existing development.
- Encourage the development of sustainable neighbourhoods that are compact, energy efficient, pedestrian-oriented and provide a mix of housing types, amenities and transportation options.
- Establish a well-designed, integrated transportation system that safely and comfortably accommodates various modes of transportation, including active transportation, between neighbourhoods, downtown areas, places to work, schools, parks and open space and other amenities and adjacent municipalities.
- Ensure development within the Town's Hamlets as focal points in the rural community that maintains and enhances hamlet character and scale.





## Sustainable Design

### Design Principles:

- Ensure the environment, social well-being and climate change mitigation is prioritized through sustainable design.
- Prioritize the creation of compact, mixed-use, complete communities where people of all ages, backgrounds and capabilities can meet their needs throughout all life stages.
- Ensure walking, cycling and other active forms of transportation are accessible, desirable, safe and well-designed to provide options for moving through the Town's built-up and rural communities.
- Recognize the interconnected system of open spaces and natural features across the Town in contributing to the health and character of communities.
- Promote sustainable development and energy conservation techniques designed to reduce greenhouse gas emissions and minimize air quality impacts.
- Encourage efficient neighbourhood, site and building design that minimizes energy consumption and the heat island effect.
- Promote design practices that conserve water and protect or enhance water quality.

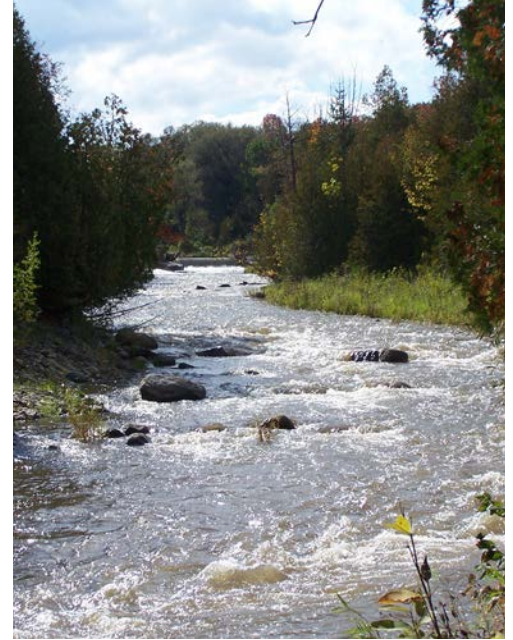




## Natural Character

### Design Principles:

- Protect and enhance the natural character of the Georgian Bay shoreline, Niagara Escarpment, Nipissing Ridge and Beaver River as unique and important features of the Town that contribute to overall quality of life, tourism, recreation and sustainable development.
- Maintain the visual quality and open space character of the Town, including significant views and vistas, when considering the design of development and redevelopment.
- Maintain a diverse and interconnected system of public open space across the Town that features convenient and comfortable access to parks, trails, natural features and shoreline areas.



## Cultural Heritage

### Design Principles:

- Recognize the contribution that cultural heritage resources make to the built-up and rural fabric of the Town's communities.
- Encourage the protection of cultural heritage resources as a tool to attract economic development, increase tourism and enhance the character and vitality of communities and neighbourhoods.
- Ensure development and redevelopment is designed to incorporate, conserve and enhance cultural heritage resources as distinct elements or community focal points.
- Encourage development that is adjacent to, or incorporates cultural heritage features, to be of an appropriate scale and character.





## Tourism and Recreation

### Design Principles:

- Embrace The Blue Mountains as a world class, four seasons recreational community by ensuring the design of the public and private realm recognizes the Town's tourism, significant natural attributes, rural countryside character and cultural heritage attributes.
- Provide for a broad range of open space settings, activities and programs to expand recreation and leisure opportunities for both residents and visitors across the Town.
- Encourage the maintenance of existing recreational uses and the establishment of new recreation amenities in the context of a year-round recreational community, particularly skiing, fishing, golfing, walking, hiking biking, camping, marinas, waterfront access and equestrian activities.
- Contribute to the active transportation network with a high quality, all-season trail system that seamlessly and efficiently connects residents and visitors to all areas and communities of the Town.

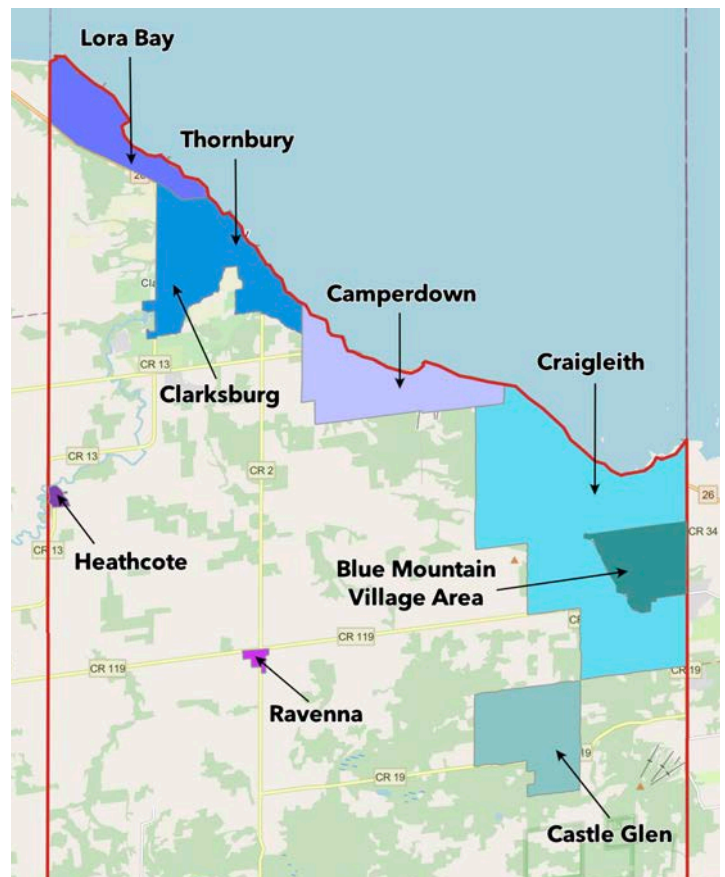


# 2

# Community Structure

As identified in The Blue Mountains Official Plan, the Town's Community Structure is made up of different Settlement Areas, or communities, each with their own unique character, identity and sense of place. As the Town of The Blue Mountains continues to grow and evolve, the Town will ensure development and redevelopment is carried out in a manner that enhances the quality and vibrancy of life in each community, for both current and future residents.

This section of the Design Guidelines provides an overview of each of the Town's communities and sets out design objectives for how specific residential, commercial and downtown areas are envisioned to evolve. Reference is made to the applicable sections of the Design Guidelines that should be referred to when proposing development or redevelopment within each community area.



Together, **Thornbury** and **Clarksburg** make up the Town's **Primary Settlement Area**, as the main concentration of residential, commercial, cultural and institutional amenities in the Town. As Primary Settlement Areas, both Grey County and the Town of The Blue Mountains direct a significant proportion of the Town's new growth to Thornbury and Clarksburg.

The Town's **Residential Recreational Areas** extend along the Georgian Bay shoreline and some inland areas. These communities accommodate a mix of seasonal and permanent residential and recreational functions. **Craigleith, Swiss Meadows, Camperdown** and **Lora Bay** are all **Residential Recreational Areas**. The Blue Mountains Village Resort Area is also identified as a Residential Recreational Area, however these Design Guidelines do not directly apply to the **Village Resort Area**.

The smaller communities of **Ravenna** and **Heathcote** are identified as **Hamlets**, which provide limited opportunities for new growth but provide housing for existing residents and serve an important commercial function for surrounding residents and the agricultural community.

Key corridors and connections such as **Highway 26**, the **Georgian Trail** and **Georgian Bay Shoreline** throughout the Town play an important role in bridging the gap between the various communities of The Blue Mountains. Protection of the Town's natural, rural and open space character greatly contributes to the quality of life enjoyed by residents and sense of place cherished by visitors.



## 2.1 Thornbury and Clarksburg

Together, **Thornbury and Clarksburg** form the Town's central hub for residential, commercial, cultural and institutional amenities.

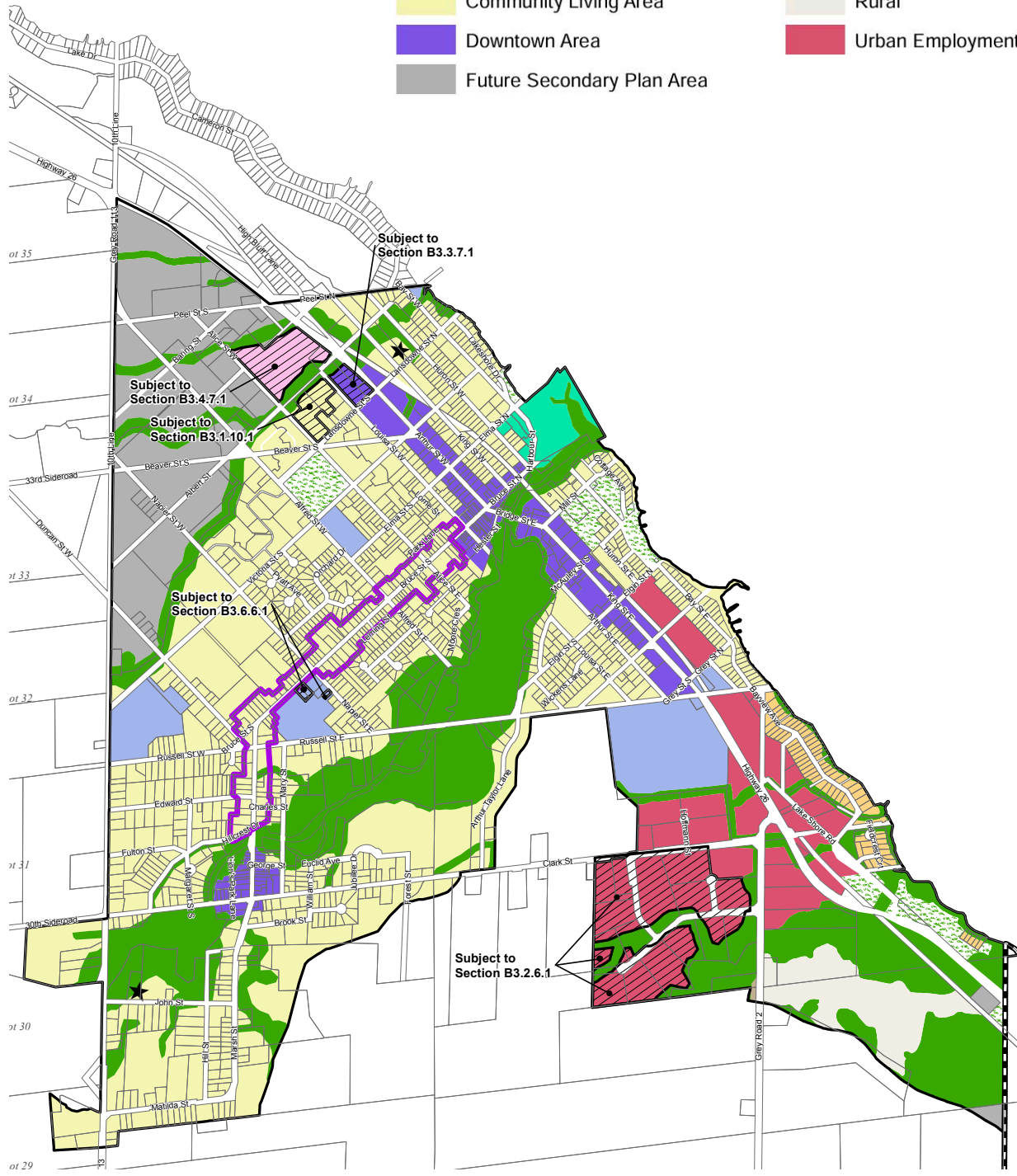


At the water's edge of Georgian Bay, Thornbury is characterized by its proximity and access to the Georgian Bay waterfront, as well as its vibrant, historic downtown core along Bruce Street boasting a variety of local boutique shops, galleries, specialty food stores and cafes. The evolving Highway 26 Corridor traverses through Thornbury, which provides a broad mix of residential, commercial, specialty service, employment, government and tourism uses in a linear fashion.

Following the Beaver River and Bruce Street South, Downtown Clarksburg is characterized by a historic main street that is home to several art galleries, local shops, specialty service uses and community spaces. Both Thornbury and Clarksburg consist of several residential neighbourhoods, each with their own predominant lot and built form characteristics. The communities of Thornbury and Clarksburg will continue to function as a place of symbolic and physical interest for both residents and visitors.



- ★ Former Landfill
- ▭ (Purple outline) Bruce St./Marsh St. Corridor
- ▭ (Black dashed outline) Niagara Escarpment Plan Boundary
- ▭ (Brown) Agricultural
- ▭ (Pink) Commercial Corridor
- ▭ (Yellow) Community Living Area
- ▭ (Purple) Downtown Area
- ▭ (Grey) Future Secondary Plan Area
- ▭ (Cyan) Harbour Area
- ▭ (Green) Hazard
- ▭ (Blue) Institutional Area
- ▭ (Green with trees) Major Open Space
- ▭ (Orange) Residential Recreational Area
- ▭ (Light Grey) Rural
- ▭ (Red) Urban Employment Area



## Thornbury and Clarksburg Land Uses

## 2.1.1 Residential Neighbourhoods

Thornbury and Clarksburg's residential neighbourhoods are designated Community Living Area in The Blue Mountains Official Plan. These areas are characterized by a small-town, neighbourhood-oriented sense of place, with a mix of established and newer homes, mature trees and walkable streets. Neighbourhoods are generally low-rise, with varied lot patterns, landscaped yards, and a comfortable relationship between buildings and the street. Access to and views of Georgian Bay, parks and open spaces further contribute to neighbourhood identity.

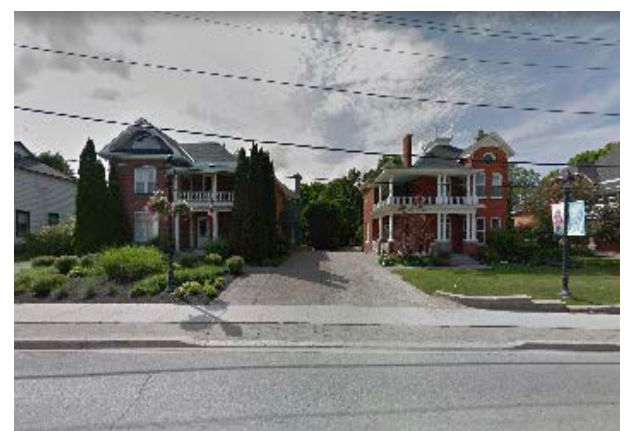
### Design Objectives:

- Enhance the small-town character of existing and established residential neighbourhoods in Thornbury and Clarksburg by ensuring development and redevelopment is compatible with the scale and density of existing development.
- Encourage the development of a broad range of housing types, from single-detached and semi-detached dwellings to townhouses and low-rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.
- Encourage gentle infill and intensification in neighbourhoods through well-designed additional residential units and converted dwellings.
- Create and maintain walkable neighbourhoods that are well connected to their surrounding downtown area, parks, open space and community amenities.
- Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 3** – Neighbourhood Design and Residential Built Form



## 2.1.2 Downtown Thornbury

Downtown Thornbury along Bruce Street is designated Downtown Area in The Blue Mountains Official Plan. Downtown Thornbury's characterized by a historic, small-town main street environment with a strong pedestrian focus and a mix of shops, services and community uses. The downtown core is defined by a low-rise built form, storefronts and cohesive streetscape elements, including sidewalks, street trees, lighting and signage. Its identity is further reinforced by strong connections to Thornbury Harbour and the Georgian Bay waterfront.

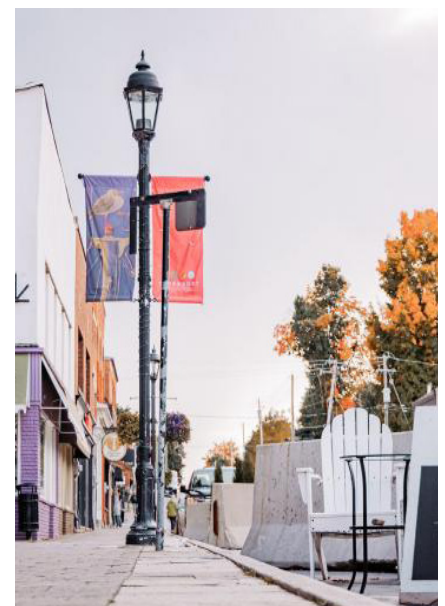
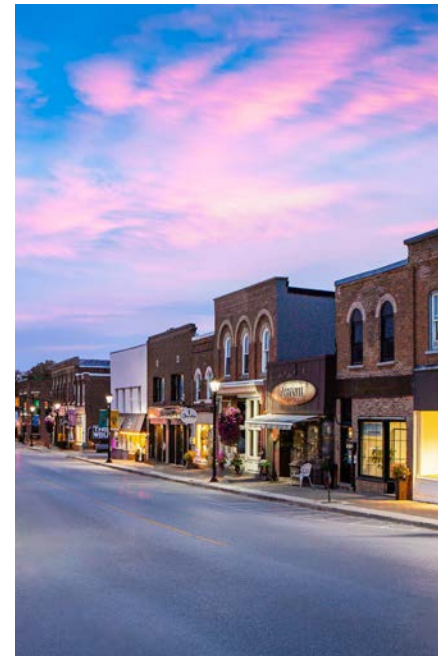
### Design Objectives:

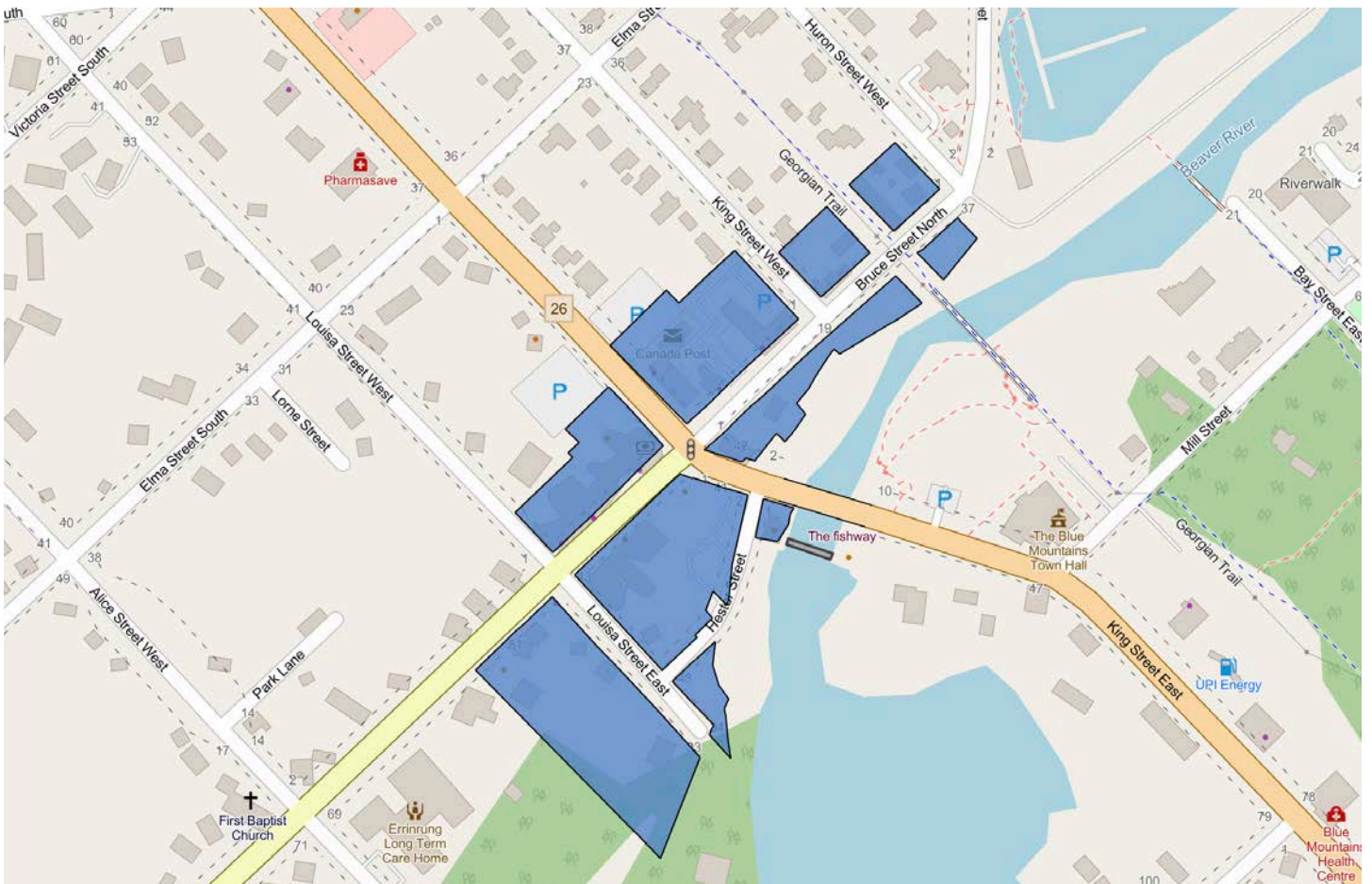
- Preserve the historic low-rise character of Thornbury's downtown core along Bruce Street.
- Ensure the scale and location of new development or redevelopment maintains and/or enhances the distinct heritage character of Downtown Thornbury.
- Reinforce the function of Downtown Thornbury as a primary cultural, business, entertainment and commercial focal point.
- Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.
- Encourage coordinated streetscape and façade improvements geared to the pedestrian that revitalize the cultural and historic character of Thornbury.
- Preserve and enhance cultural and historic features that exist within Downtown Thornbury, including the restoration and enhancement of building facades.
- Create new community focal points in Downtown Thornbury by encouraging the creation of new public spaces, pedestrian connections and public art.
- Enhance the pedestrian experience and connections along Bruce Street from the downtown core to Thornbury Harbour and the Georgian Bay waterfront.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 4** – Downtown and Commercial Areas





### Thornbury Downtown Core



## 2.1.3 Highway 26 Through Thornbury

The stretch of Highway 26 through Thornbury, from Grey Street to Albert Street, is designated Downtown Area in The Blue Mountains Official Plan. Highway 26 through Thornbury functions as a key gateway corridor, characterized by a mix of commercial, service and residential uses within a more open, auto-oriented setting. The corridor is defined by varied building setbacks, larger lot patterns and generally low-rise built form, with scattered landscaping and views to surrounding neighbourhoods and natural features. A broad range of uses is permitted, with residential uses encouraged within mixed-use buildings, while stand-alone residential development is directed to side streets to maintain the corridor's mixed-use character.

### Design Objectives:

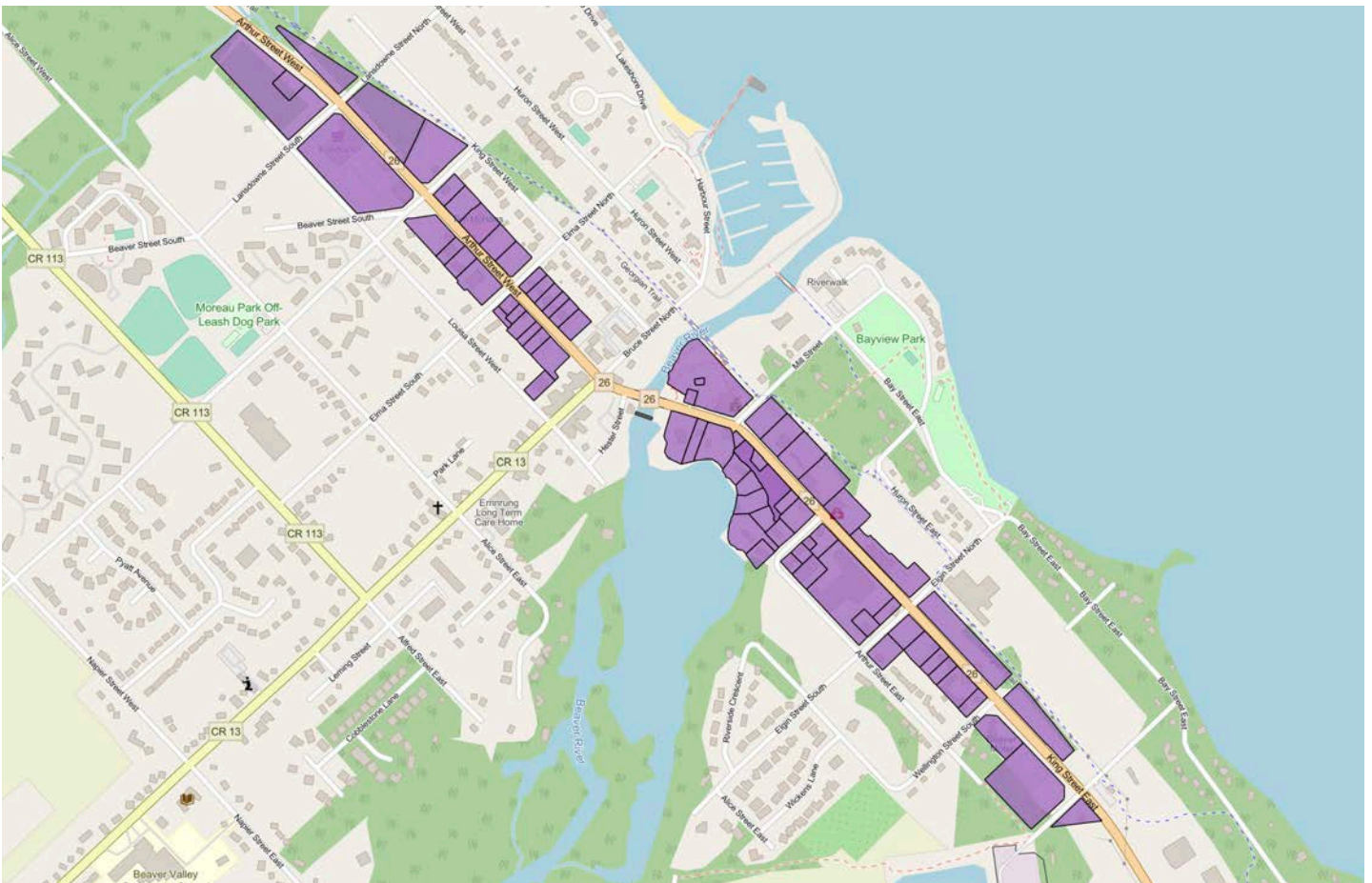
- Establish Highway 26 through Thornbury as a priority area for mixed-use development, redevelopment and intensification.
- Ensure development fronting onto Highway 26 is held to a high standard to building, landscape and site design, as the main gateway to and from Thornbury.
- Encourage the development of a broad mix of uses, including residential, to provide access to range of amenities, as well as vibrant and safe public places within walking distance.
- Ensure the low-rise character of adjacent neighbourhoods and Downtown Thornbury is protected through compatible intensification and appropriate transition.
- Maintain and enhance the open space character of properties along Highway 26 and ensure the continued and improved feeling of spaciousness through development and redevelopment.

### Volume 1 Guidelines:

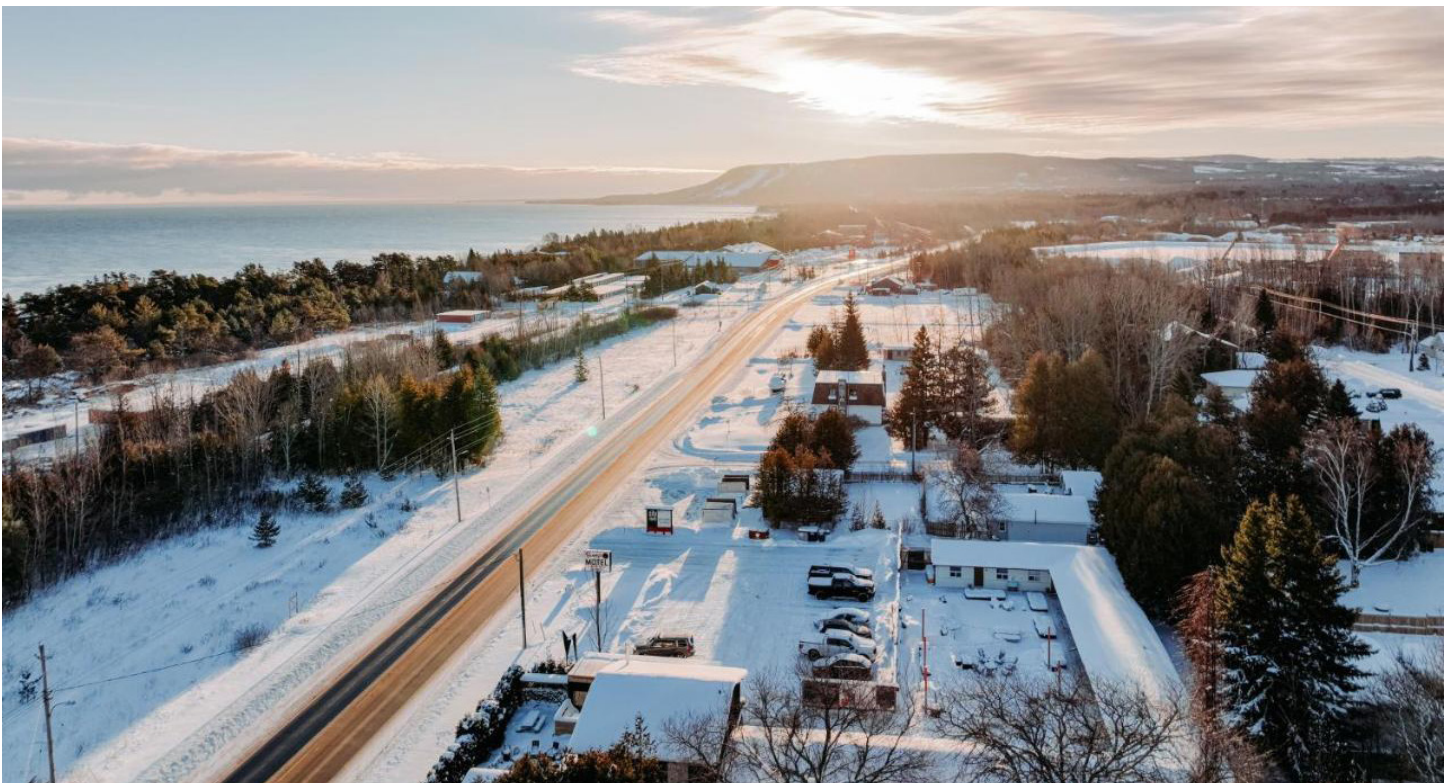
**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 4** – Downtown and Commercial Areas





## Highway 26 Through Thornbury



## 2.1.4 Downtown Clarksburg

Downtown Clarksburg along Marsh Street and Clark Street is designated Downtown Area in The Blue Mountains Official Plan. Downtown Clarksburg is characterized by a small-town main street environment with a strong arts and cultural identity, a mix of local shops, services and community uses and a pedestrian-oriented streetscape. The downtown features low-rise buildings, heritage storefronts and streetscape elements, including sidewalks, street trees, lighting and signage. Maintaining and enhancing the identity of Clarksburg through sensitive infill and redevelopment is essential to supporting Downtown Clarksburg's role as a vibrant, arts-focused community hub.

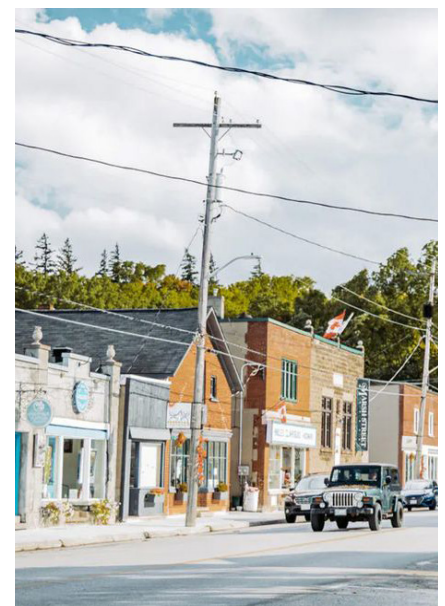
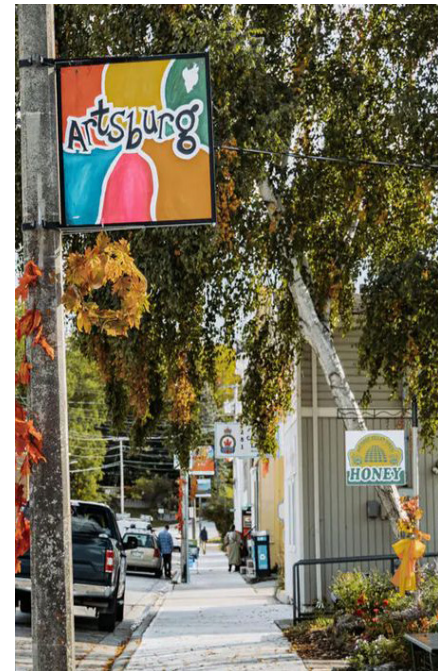
### Design Objectives:

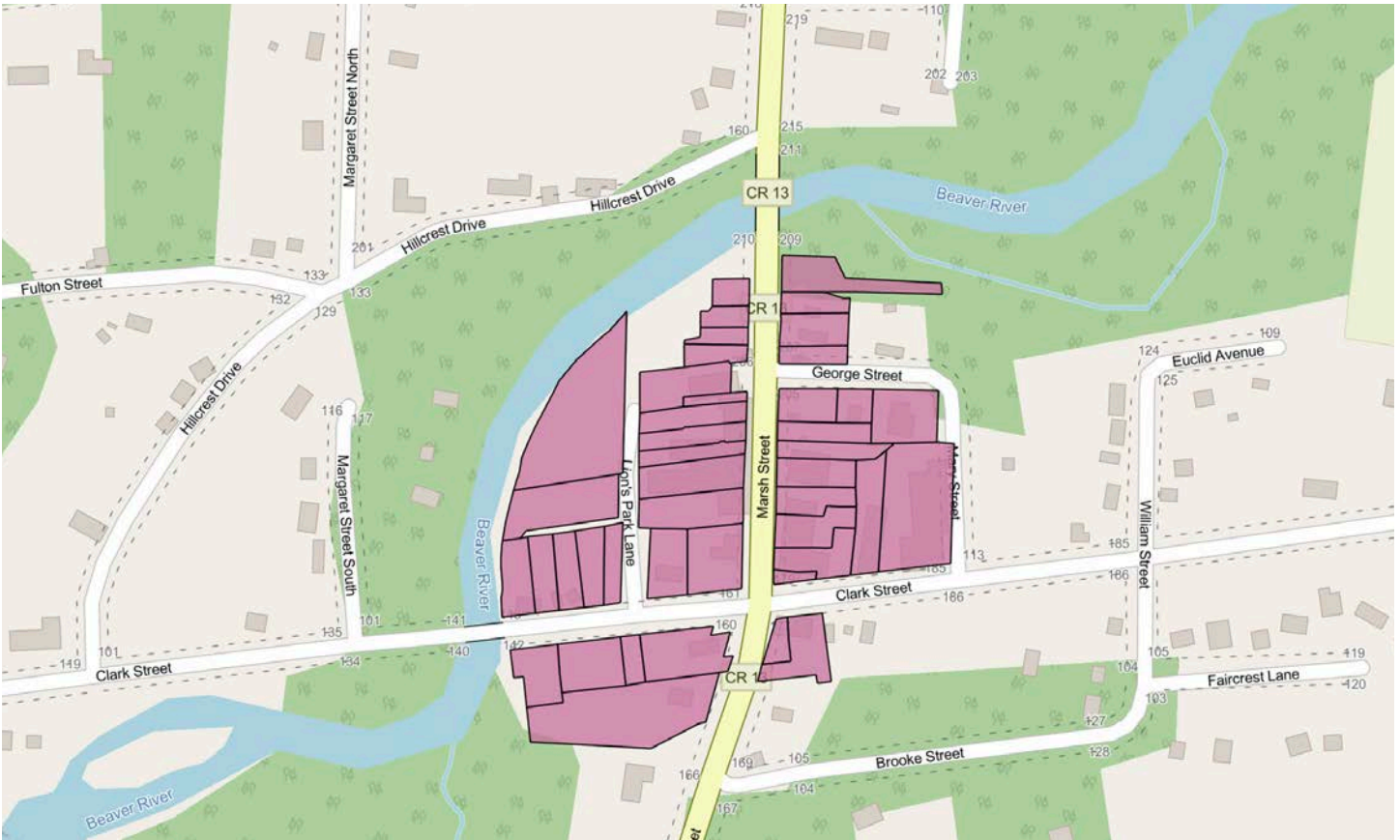
- Reinforce the vibrancy of Downtown Clarksburg as a local, vibrant community with a strong sense of place tied to arts and culture.
- Ensure the scale and location of new development or redevelopment maintains or enhances the built form and streetscape character of Downtown Clarksburg.
- Reinforce the function of Downtown Clarksburg as a mixed-use focal point for the surrounding community, accommodating cultural, business, entertainment, commercial and service uses.
- Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.
- Encourage coordinated streetscape and façade improvements geared to the pedestrian that build on the sense of place for Clarksburg.
- Preserve and enhance cultural and historic features that exist within Downtown Clarksburg, including the restoration and enhancement of building facades.
- Create new community focal points in Downtown Clarksburg by encouraging the creation of new public spaces, pedestrian connections and public art.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 4** – Downtown and Commercial Areas





Downtown Clarksburg



## 2.1.5 Urban Employment Area

A variety of employment, industrial and manufacturing sites are located at the eastern edge of the Thornbury/Clarksburg Settlement Area, with predominant access from Highway 26 and Grey Road 2. The Blue Mountain Official Plan designates these lands as Urban Employment Area, which recognizes lands currently developed for employment uses, as well as lands identified to be the site of employment uses in the future.

The Urban Employment Area designation permits a range of manufacturing, assembly, processing and fabrication uses, as well as storage and warehousing, wholesaling establishments and similar uses.

### *Design Objectives:*

- Prioritize opportunities for a diverse range of existing and new businesses to grow and develop in Thornbury in strategic locations.
- Ensure employment area development fronting onto the public realm is held to a high standard of building, landscape and site design.
- Ensure employment and industrial uses are adequately buffered and screened from any adjacent residential, open space or recreational use.

### *Volume 1 Guidelines:*

#### **Section 5 – Urban Employment Areas**



## 2.2 Craigleith

**Craigleith** is designated Residential Recreational Area in The Blue Mountains Official Plan, providing both a seasonal and permanent residential and recreational function.



Situated between the Georgian Bay shoreline and the Niagara Escarpment, Craigleith is a residential community rooted in natural beauty and recreation. Craigleith is defined by its natural and recreational amenities across the community including ski clubs, golf courses and the waterfront, as well as Craigleith Provincial Park along Georgian Bay. Craigleith accommodates a variety of residential neighbourhoods and areas, from well-established chalet-style dwellings to newer subdivision development south of Highway 26.

Highway 26 serves as a significant linear connection through Craigleith, providing a few commercial and tourism-related uses along the way. Over time, Highway 26 is envisioned to accommodate a broader mix of commercial and service uses that bridge the gap between Collingwood and Thornbury. Craigleith Village, located at the eastern edge of the Town along Highway 26 is also envisioned to be redeveloped into a sustainable, compact community that accommodates a mix of residential and commercial uses and creates a new central node for existing Craigleith residents and visitors.



- Agricultural
- Major Open Space
- Commercial Corridor
- Recreational Commercial Area
- Craigleith Village Commercial
- Recreational Ski
- Craigleith Village HSFPSW
- Residential Recreational Area
- Escarpment
- Resort Commercial
- Future Secondary Plan Area
- Rural
- Craigleith Village Residential
- Special Agricultural
- Hazard
- Wetlands
- Institutional Area
- Niagara Escarpment Plan Boundary



## Craigleith Land Uses

## 2.2.1 Residential Neighbourhoods

Craigeleith's variety of residential areas are designated Residential Recreational Area in The Blue Mountains Official Plan. Craigeleith is characterized by a recreational, landscape-oriented sense of place shaped by its location between the Georgian Bay shoreline and the Niagara Escarpment. The community is defined by its strong connection to natural features and recreational amenities. Residential areas include a mix of established chalet-style homes and newer neighbourhoods, generally set within a low-rise built form and a more open, natural setting. Residential development should build on Craigeleith's natural setting and recreational character as the community continues to evolve.

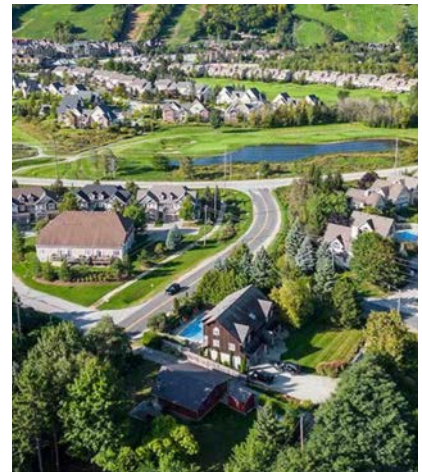
### Design Objectives:

- Enhance the character of existing residential neighbourhoods in Craigeleith by ensuring development and redevelopment is compatible with the scale and density of existing development.
- Encourage the development of a broad range of housing types and tenures, from single-detached and semi-detached dwellings to townhouses and low-rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.
- Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
- Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space and community amenities.
- Prioritize pedestrian-oriented design, cycling facilities and trail connections throughout Craigeleith.
- Reinforce the uniqueness of each neighbourhood, with a focus on distinct heritage characteristics, civic and gathering spaces, natural features and areas, recreation amenities and accessibility.
- Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 3** – Neighbourhood Design and Residential Built Form



## 2.2.2 Craigleith Village

The Craigleith Village Community, located at the eastern edge of the Town along Highway 26, is an area prioritized to redevelop into a sustainable, compact mixed use community while protecting the character of the surrounding area. The Blue Mountains Official Plan divides Craigleith Village into sub-designations, including Village Commercial, Village Residential and Hazard Lands, Shoreline Floodplain and Provincially Significant Wetlands. The Craigleith Village Commercial designation intends to function as a primary commercial centre to serve the community of Craigleith and traveling public. Residential dwelling units are encouraged in the upper portion of mixed use buildings. The Craigleith Village Residential designation identifies lands where a compact residential community will be established on the shores of Nottawasaga Bay, together with complimentary recreational lands and facilities.

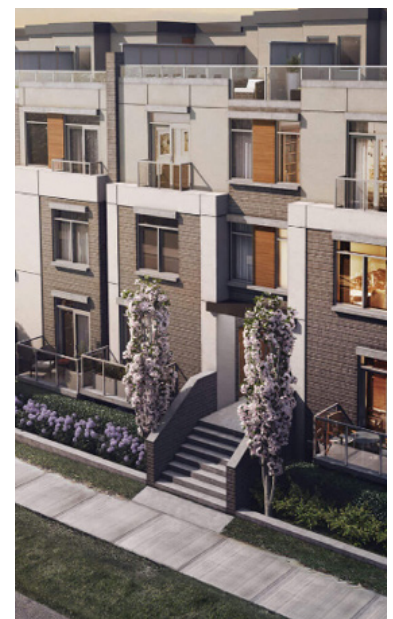
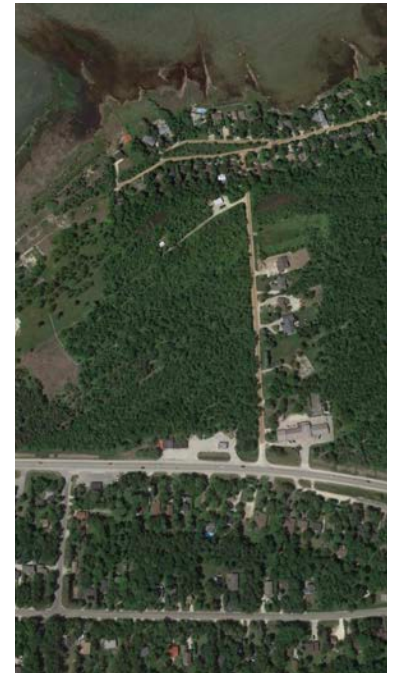
### Design Objectives:

- Develop a strong identity for Craigleith Village that accommodates a mix of residential, commercial and institutional uses and creates a new central node for residents and visitors.
- Ensure the scale and pattern of development in Craigleith Village respects surrounding residential lands and provides an appropriate transition.
- Encourage redevelopment in Craigleith Village to maximize residential intensification opportunities and variety through building sizes and inclusion of residential units above commercial uses.
- Encourage new coordinated streetscapes geared to the pedestrian, as well as new community focal points including public squares, that create a unique sense of place for Craigleith Village.
- Ensure the community road and trail network provide residents with a safe, functional and attractive neighbourhood that offers access to sustainable natural areas including wetlands, tree cover and the shoreline.
- Encourage the development of new recreation opportunities are tied to the environment and active, healthy living.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 4** – Downtown, and Commercial Areas



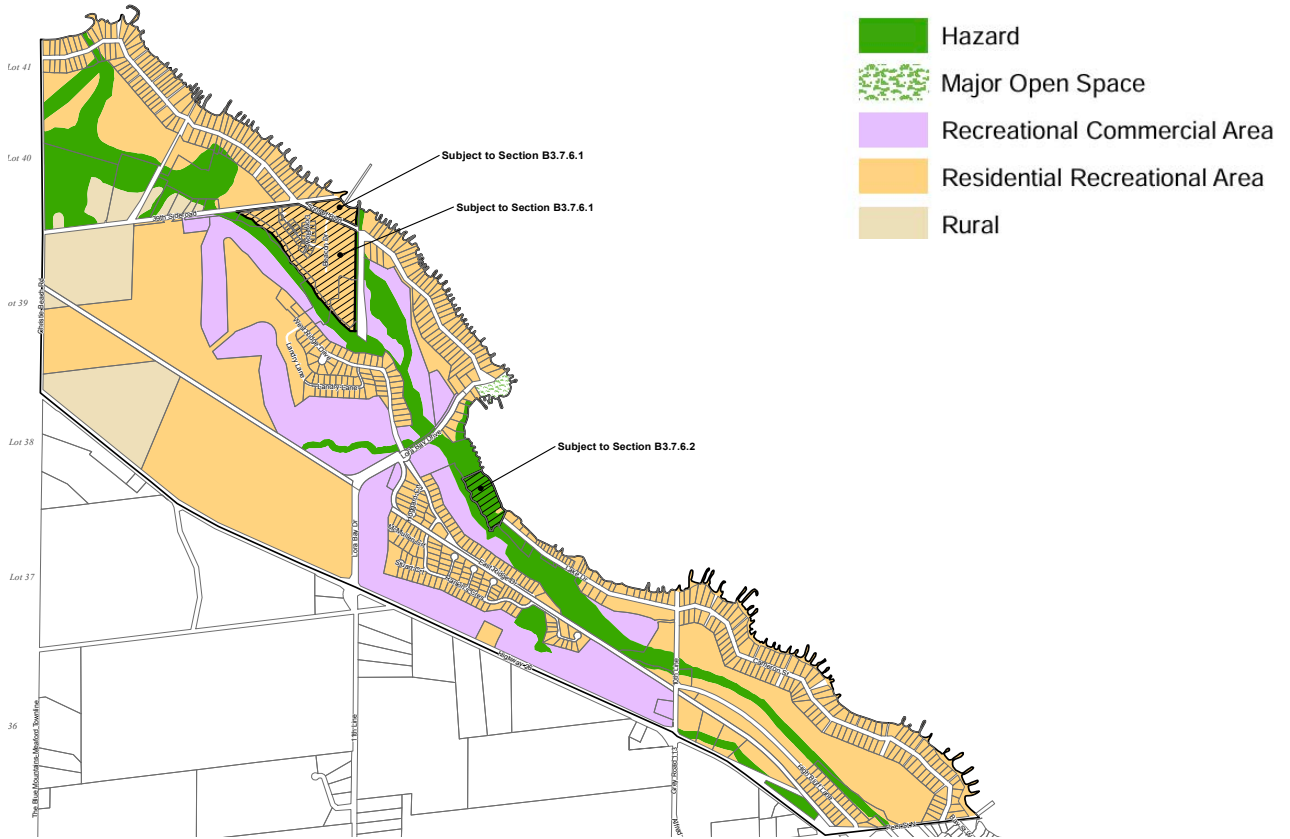
## 2.3 Lora Bay

**Lora Bay** is designated Residential Recreational Area in The Blue Mountains Official Plan, providing a seasonal and permanent residential and recreational function.



Lora Bay is a master planned, resort-like residential community located just west of Thornbury, along the shore of Georgian Bay. The Lora Bay community has continued to develop over time through multiple phases of subdivision development. Centred around the Lora Bay Golf Course, the open space character of the community is defined by its location along the water, as well as a series of public parks and private open spaces. The community primarily consists of single-detached residential units, with multiple residential units dispersed throughout.

The Residential Recreational Area designation permits a range of low-rise residential uses, as well as parks, open spaces, trails, community centres, cultural facilities, recreational clubs and other similar day uses. Lora Bay's residential areas are characterized by predominantly low-rise dwellings, including single and semi-detached and townhouse units, that share a coordinated architectural style and cohesive design language. Well-landscaped streetscapes and consistent built form contribute to a unified community character, centered around an amenity-rich open space environment.



### Lora Bay Land Uses

## 2.3.1 Residential Neighbourhoods

### Design Objectives:

- Encourage development that is in keeping with the evolving open space and built form character of Lora Bay.
- Encourage a mix a broad range of housing types in appropriate locations while maintaining compatibility with existing adjacent character.
- Encourage gentle infill in neighbourhoods through well-designed additional residential units.
- Ensure neighbourhoods are well connected to parks, open space, the waterfront and community amenities.
- Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 3** – Neighbourhood Design and Residential Built Form



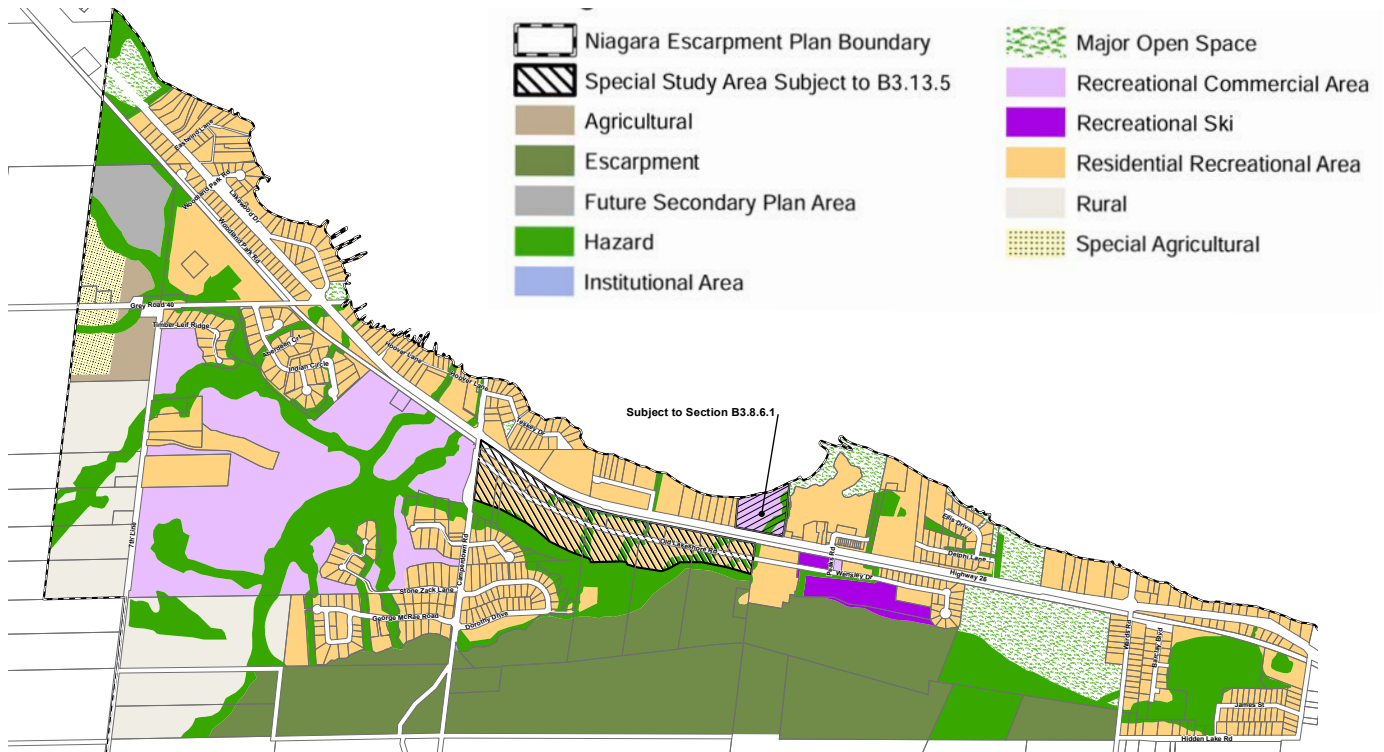
## 2.4 Camperdown

**Camperdown's** residential areas are designated Residential Recreational Area in The Blue Mountains Official Plan.



Camperdown is a primarily residential community immersed in the natural beauty between the Georgian Bay and Niagara Escarpment. The community is characterized by its surrounding ski clubs, golf courses, natural areas and considerable open space areas. Camperdown accommodates a mix of established and new residential dwellings north of Highway 26 along the edge of Georgian Bay, as well as several new estate residential dwellings and subdivisions south of Highway 26. An eclectic mix of dwelling and neighbourhood types defines the unique, varied character of Camperdown that should be celebrated and enhanced through any development or redevelopment.

The Residential Recreational Area designation permits a range of low-rise residential uses, as well as parks, open spaces, trails, community centres, cultural facilities, recreational clubs and other similar day uses. Camperdown's residential areas are characterized by a varied and evolving built form, including a mix of estate lots and newer subdivisions. Streetscapes are generally more rural in character while architectural styles and lot patterns are diverse. Development is unified by its strong relationship to the surrounding natural landscape, including views, topography and adjacent open spaces. This contributes to a distinct and flexible residential character that responds to its natural setting and lifestyle.



## Camperdown Land Uses

### 2.4.1 Residential Neighbourhoods

#### Design Objectives:

- Ensure new residential development reinforces the uniqueness of each neighbourhood area in Camperdown, including the prominent lot pattern, scale, built form characteristics, landscape and open space character.
- Encourage a broad range of housing types while maintaining compatibility with existing character.
- Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
- Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space, the waterfront and community amenities.
- Prioritize pedestrian-oriented design, cycling facilities and trail connections.

#### Volume 1 Guidelines:

**Section 2** - Environment, Parks, Public Spaces and Culture

**Section 3** – Neighbourhood Design and Residential Built Form



## 2.5 Villages and Hamlets

**Heathcote** and **Ravenna** are formally identified as Hamlets, or Secondary Settlement Areas, by Grey County and The Blue Mountains Official Plan.



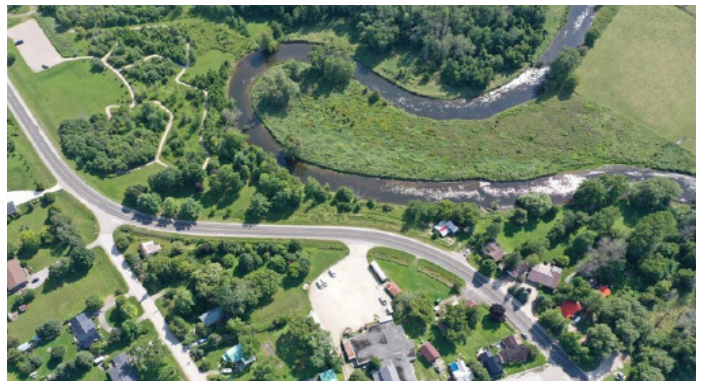
Interspersed along and atop the Niagara Escarpment is a collection of the Town's Villages and Hamlets. The Town's Hamlet Areas are existing communities with residential, commercial and recreational uses that function as small central places within the Town. These rural areas offer natural view corridors, local stores and markets, restaurants and farm-related uses.

The rolling hills and undulating topography, rivers and streams and open setting of the Town's rural landscape are all components that define its character. Development will be limited across the Town's rural communities and must be compatible with the character, role and function of the rural landscape.

## 2.5.1 Villages and Hamlets

### Design Objectives:

- Ensure any new development or redevelopment is compatible with the character, role, scale and function of the surrounding natural, agricultural and rural landscape.
- Ensure Hamlets are maintained as focal points for the surrounding rural community and tourism with a mix of diverse and compatible uses in proximity to each other.
- Protect and enhance the natural character of the Niagara Escarpment as a unique and important feature of the Town.
- Carefully control new residential development in Hamlet Areas in order to maintain existing scale and character.
- Provide opportunities for small-scale commercial and tourism related uses that are compatible with the character and scale of these areas.
- Preserve and enhance the natural and historic features that exist in each rural community.






**The Blue Mountains Community Design Guidelines Comments – April**

Commentor	#	Comment	Response	
Brian Nelson	<b>Volume 1</b>			
	1	Section 1.4: I strongly support a requirement for development proponents to submit “a Community Design Checklist and/or supporting materials or studies to confirm that the applicable Design Guidelines have been considered and incorporated into development proposals”. Documentation will enable members of the public, as well as Staff and Council, to understand if/how proposals conform to the Guidelines, and advocate for conformity where possible. At the public engagement session held on April 14, 2026, PDS staff and consultants outlined how the Design Guidelines can be used in development approval processes, including pre-consultations with proponents, which would appear to serve as opportunities to introduce a Design Checklist. Any efforts by staff and Council to ensure that the Guidelines are applied in practice will be very important.	Thank you for the comment.  A draft checklist has been prepared as part of the final Community Design Guidelines to be implemented through development application review.	
	2	Section 2.1.1: A definition of “woodlot” may be required to clarify and ensure that the Guidelines apply broadly to any significant stand of trees.	The Town’s Official Plan defines “woodlands”. The section and language in the Guidelines has been revised to “woodlands” for consistency.	
	3	Section 2.1.3: Reference to the Town’s Municipal Tree Preservation By-law is very important. A major public effort to ensure adoption of a long overdue updated Tree Preservation By-law following the upcoming municipal elections is anticipated.	Noted. An additional reference to the Tree Preservation By-law has also been added to Section 1.4 as part of the list of documents to also be referenced.	
	4	Section 3.1: A stronger reference to the importance of storm water management is recommended, given the significance of this issue in our Town. This should include insuring adequate open space for absorption of stormwater in both public spaces and private lots, as well as protection of watercourses.	In addition to the stormwater management guidelines in Section 2.4, an additional guideline in Section 3.1 has been added to ensure development parcels provide sufficient public and private landscaped open space to support stormwater infiltration, retention and runoff management through parks, natural features, stormwater management facilities and on-site landscaped areas.	
	5	Section 3.2: Guidelines supporting the incorporation of energy efficiency measures into building design and construction should be added throughout this section (e.g., passive/active solar, these should address all major building types (i.e., single detached, semi-detached, townhouse, infill/replacement housing, ARUs, low-rise residential buildings).	A new Section (3.2.6) has been added with a new set of guidelines for resilient and energy efficient built form based on comments received.	
	6	Section 4.1: Stronger and more direct references to design features that support pedestrian-friendly spaces, active transportation, and public safety are recommended. These features are increasing recognized globally as critical to the success of downtown areas and should be prioritized over motor vehicle traffic flow and parking where necessary.	Additional language and guidelines focused on pedestrian-friendly spaces, active transportation and safety have been added throughout this section.	
	7	Section 5.1: Add guidelines to support/encourage energy efficiency measures and renewable energy systems in site and building design.	Supporting guidelines have been added to Section 5.1 and 5.2 for Built Form and Site Design.	
Brian Nelson	<b>Volume 2</b>			
	8	Section 1.2: Sustainable Design: I attended the Energy Mapping Workshop sponsored by the Town on February 11, 2026. Several themes were discussed that potentially relate to Town structure and design such as renewable energy, active transportation, public transportation, and intensification of development. An example would involve a network of energy hubs in strategic locations across Town. These would be energy generation hubs based on renewable sources. A key role of these hubs would be to serve as charging stations for a fully electrified fleet of transit vehicles. The energy hubs could also support active transportation connecting to our trail systems and providing services such as bike and e-bike rentals. They could also serve as focal points for higher density, multi-use developments and much needed housing alternatives in key locations. There is potential for large-scale renewable energy and district heating projects that could improve energy efficiency and reduce costs. As energy hubs evolve into more intense and diverse land use hubs the benefits circle back to transportation by reducing travel distances and vehicle trips. Ideas like this may not fit directly into Design Guidelines or other planning tools at this time. However, envisioning how a network of energy hubs can be integrated into our settlement areas could potentially lead to	Noted. No further changes required at this time	

		practical solutions to key planning challenges in our Town such as providing economically viable public transportation, managing traffic on Highway 26, and reducing our carbon footprint.	
	9	Section 1.2: Natural Character: This section should be strengthened by referencing how our natural assets are central to the identity of our Town, our quality of life, our tourism and recreational industries, our economy, and our capacity to ensure sustainable development.	Wording has been added to the end of Natural Character Principle #1.
	10	Section 2.2: Stronger support for commercial and institutional uses is recommended to address the longstanding need for services to support extensive residential development in this area.	Noted. The specific section and objectives for Craigleith Village is intended to signify this. Additional context has also been added to the new Official Plan reflecting this need.
Grey County Planning and Development Cassandra Dilman – Intermediate Planner	<b>General Comments</b>		
	11	Grey County’s population is growing and aging faster than the provincial average. As noted in the Official Plan, this presents an opportunity to adopt an Age-Friendly approach to planning, design, and decision-making. The built environment has significant influence towards creating and maintaining a healthy lifestyle, which includes identifying and responding to age- and dementia-friendly needs through supportive infrastructure and services for residents of all ages (4.3.1).	Noted.
	12	The OP also encourages parks to be designed or redesigned as accessible, Age-Friendly spaces that provide a balance of passive and active recreational options (7.12.5). Further, new subdivision development should incorporate Age-Friendly and transit-supportive elements, such as connected pedestrian and cycling networks, complete streets, active transportation features, and enhanced safety (8.5.4).	Noted. This is addressed through the neighbourhood design guidelines.
	13	The County’s Age Friendly Community Strategy builds upon these policies to consider an Age-Friendly approach to community building and development. For example, housing choices, such as smaller rental apartments, allow people to live independently for as long as possible and remain part of their communities as they age.	Noted.
	14	County staff acknowledge and appreciate that many of the proposed design guidelines already incorporate Age-Friendly considerations (e.g., Sections 2.3.3, 2.2.4.4, 3.2.5.16, and 4.1.6.5). Staff would encourage the continued integration of an Age-Friendly lens throughout the guidelines. Applying an Age-Friendly and dementia-inclusive approach to community design can support positive intergenerational outcomes at the neighbourhood, street, and individual unit scales, helping to create communities where people of all ages—children, youth, adults, and older residents—can live, work, and play.	Noted. Additional references to age-friendly have been added in Section 2.2 (Parks, Open Space and Waterfront Areas) and 4.1 (Downtown and Commercial Areas).
	15	Staff note that there may be opportunities to further strengthen the guidelines through more explicit language related to Age-Friendly and dementia-inclusive design. For example, Guideline 3.1.1.6 could be interpreted through a dementia-inclusive lens, as retaining heritage structures can provide familiar, recognizable, and prominent landmarks that support people’s mental mapping of neighbourhoods and enhance the street experience for individuals living with dementia.	Age-friendly language has been added throughout the document.
	<b>Examples of design considerations from an Age-Friendly perspective may include the following:</b>		
	16	• Incorporating weather protection elements such as awnings and overhangs, particularly in conjunction with seating at building entrances, to provide comfortable shelter for at least two people waiting together.	Noted. Addressed in Section 4.2.2 and 4.2.3, as well as a new added Section 3.2.6 Resilient and Energy Efficient Building Design.
	17	• Encouraging lay-by parking in close proximity to building entrances to support accessible pick-up, drop-off, and delivery functions for older adults and families.	An additional guidelines has been added to Section 3.2.5 (Low Rise Residential) Section 4.3.2 (Commercial/Mixed Use Site Design).
	18	• Designing play spaces that are welcoming and inclusive for a wide range of users, including children, older adults, caregivers, and people with disabilities.	An additional guidelines has been added to Section 2.2 (Park Design).
	19	• Considering the full spectrum of the sensory experience, including visual cues and sound. For example, certain ambient sounds (such as children playing, birdsong, or running water) may be beneficial for people living with dementia, while high traffic noise or large crowds can be overwhelming.	This relates more to detailed design of parks, open spaces, etc. than needed for the Guidelines.
	20	• Providing seating with a traditional, easily recognizable bench form to clearly signal places for rest.	Reference to “easily recognizable” has been added to the benches guideline in Section 4.1.4 (Street Furniture).
	21	• Locating waste storage areas within multi-unit residential buildings, where feasible, to improve accessibility and ease of use.	A guideline has been added to Section 3.2.5 (Low Rise Residential Buildings – Site Design).
<b>County Climate change staff have reviewed the proposed guidelines and provided the following comments:</b>			

	22	Grey County Climate Change Staff see significant alignment with climate change policies in Grey County's Official Plan (s. 7.13) and priority actions in Going Green in Grey, the County's climate change action plan, such as protecting and expanding forest, habitat, and biodiversity, accelerating zero-emission vehicle adoption and active transportation, and promoting compact, mixed-use development. Several guidelines align with the recently approved voluntary Green Development Program (i.e., Native Species, Tree Canopy, Park Access, EV Readiness, Complete Streets, Solar Readiness, Energy Efficiency and Electrification).	Noted. Thank you.
	23	a) County staff support energy-efficient building design of community spaces (s. 2.5.8) and support potential expansion to residential buildings.	Noted.
	24	b) Grey County's climate change adaptation planning process projects significant increase in extreme heat days (+30C). Therefore, County staff see strong rationale for heat island mitigation in parking areas (s.4.3.2.11) and see potential expansion to include similar measures for roofing materials, or incorporating other resilience measures, such as flood or wind proofing measures.	A new Section 3.2.6 (Resilient and Energy Efficient Building Design) has been added. A new guideline has been added to encourage the use of green roofs, white roofs or other high-reflectance and energy efficient roof treatments.
	25	c) County staff appreciate reference to building materials, and there may be an additional opportunity to encourage use of low-carbon building materials, in support of the Embodied Carbon Metric of the Green Development Program.	A new Section 3.2.6 (Resilient and Energy Efficient Building Design) has been added. A new guideline has been added to encourage the use of durable, low-maintenance and low-carbon building materials.
	26	d) Lastly, in support of both climate change and Age-Friendly strategies, the Town may consider additional guidelines to support transit readiness.	The Guidelines are intended to support walkability, gentle intensification and improved streetscape design and connectivity. An additional transit infrastructure guideline has been added to Section 4.1 (Downtown Streetscapes).
	27	In addition to the above staff appreciate and acknowledge the following alignment with the Green Development Program: <ul style="list-style-type: none"> <li>• Prioritizing sustainable design features, such as low-impact design (LID) measures, as it aligns with the Stormwater Metric.</li> <li>• Encouraging use of native, non-invasive plant species aligns with the Native Species Metric.</li> <li>• Ensuring parks have street frontage (s.2.2.6) aligns with Park Access Metric.</li> <li>• Orient buildings and lots to maximize solar access and opportunities for passive solar design (s. 3.1.2.5), as it aligns with our Solar Readiness Metric.</li> <li>• Use of Dark Sky Compliant lighting aligns with the Bird Friendliness/Dark Sky Metric.</li> </ul>	Noted. Thank you.
<b>County Ecology staff have reviewed the proposal and provided the following comments:</b>			
	32	Staff generally have no concerns with the proposed guidelines and have one question for clarity regarding Guideline 2.1.1 Woodlots: <i>3. Ensure that woodlots have at a minimum, 50% public frontage and visibility, through such means as single loaded streets or incorporation into a park or stormwater management area.</i>  Staff are unsure of the intent behind this guideline section and note that it may not be a feasible goal. Please clarify the intent of this guideline. i.e., is this intended to target 50% of the woodland dripline, and for this dripline to be visible and also front a public road, park or stormwater management system?	The intent of the Guideline is to ensure visibility and safety by making the frontage of a woodland largely visible. It is not intended to be a technical standard and can be applied in a flexible manner.
	34	The County requests notice of any decision rendered with respect to this file.	Noted.
<b>Great Gulf</b> Trish Elliott – Development Manager, Land Development	35	My primary concern is that we need to better understand how the Guidelines will be interpreted and integrated into the development review process. Can you please provide more details on this?  The reason I ask is that the document speaks to these being guidelines and not policies/regulations (so that they can be applied in a flexible manner), yet there are many statements in the document that use the word “ensure”, “will” or “shall”. This language is in conflict and needs to be addressed, otherwise we will have significantly more comments on areas where changes are needed to maintain the intended flexibility in the guidelines.	The concern is understood. The few references to “shall” and “will” have been replaced throughout the document. Language has been added to the beginning of the document – Section 1.4, to note that the guidelines will be interpreted and applied flexibly, with action words serving to indicate the Town's ultimate objectives rather than prescribe rigid requirements.

	36	Some examples of this are: 1) The use of “ensure” in Volume 1, Section 2.1.1 (Woodlots), Item #3 means that developments are required to meet this threshold for public frontage/visibility, however the site specific context of a development (e.g. the organic edge of the natural heritage limit, the topography of the land, the size/shape of land beside the NHS, etc.) may not facilitate this.	Ensure has been revised to encourage.
	37	2) The use of “ensure” in Volume 1, Section 3.2.1 (Lot Configuration and Building Orientation), Item #2 does not allow for creativity in lot siting or site-specific conditions. Things such as a unique lot shape, creation of a focal point or architectural interest, etc. may mean that a parallel orientation for the home is not necessary. So long as the building is addressing the street and creating the desired interest/pedestrian friendly condition, this creativity should not be discouraged.	The reference is Section 3.1.2. The wording has been revised; however it is important to note that these are general guidelines and cannot be expected to address all unique situations.
	38	We also have a handful of guideline-specific comments: - Volume 1: Under the “Orientation, Massing and Articulation” portion of 3.2.1, Item #6 speaks to the design of corner lots and appears to be the same as Items #6 and #7 under Section 3.2.2 (Townhouse Dwellings). We recommend that the existing #6 be replaced with the language used for #6 & #7 in the Townhouse section, as these contain a bit more clarity in direction.	Section 3.2.1 has been revised.
	39	Volume 1: Under the “Driveway and Garages” portion of Section 3.2.1 (Single Detached and Semi-Detached Dwellings) and Section 3.2.2 (Townhouse Dwellings), Item #2 and #12 speak to the driveway width being limited by the width of the “interior of the garage”. We would like to suggest that the wording be changed to just “the width of the garage”, as it can be quite technical to determine the inside vs. outside of the garage, and even the images used on this page appear to show driveways that span the full garage instead of just the limit of the inside wall of the garage. The current wording could also create situations where there are awkward gaps in the pavement (that cannot facilitate grass), such as the narrow width of the wall that divides the interior garage of these 2 townhouse units (image taken from page 35 of Volume 1):  	Noted. The wording has been revised to remove interior.
	40	Volume 1: Under the “Driveways and Garages” portion of Section 3.2.1 (Single Detached and Semi-Detached Dwellings), Item #6 that speaks to driveways being from the ‘secondary street’ should include the additional clarification language in Section 3.2.2 Item #15 to define what this means.	Noted. The same wording from Section 3.2.2 has been added to Section 3.2.1.
<b>Town Roads and Drainage</b>	<b>Stormwater Management (Page 19 – Section 2.4)</b>		
	41	Section 2.4 could be reviewed for consistency with the Town’s Stormwater Management By-law, particularly with respect to how stormwater management facilities are described in relation to use and access.	Noted. Changes have been made

	42	The guidelines encourage stormwater management facilities to function as passive open space amenities. The Town's by-law restricts direct access and contact within the facility for safety reasons. References to "recreational" should be removed to remain consistent with the By-law, which restricts public interaction with stormwater management facilities.	Noted. "Recreational" has been removed. FYI, a lot of the "green amenity" and pedestrian connection language was carried forward from the town's original Community Design Guidelines.
	43	• Pt. 3: it will be helpful to clarify that the 1.8 metre mow strip is intended to be located on private property and not on Town owned lands.	This was not the intent. The Guideline has been removed for clarity.
	44	Pt 6: The word "decorative" should be removed. Fencing, where required, should be functional and consistent with safety requirements and Town standards.	"Decorative" has been removed.
	<b>Driveways and Garages (Page 33)</b>		
	45	Pt 3: this bullet point should be removed.	The Guideline has been removed.
	<b>Street Trees and Plantings (Page 50)</b>		
	46	• Pt 4: please remove the reference to 8 to 10 metre spacing. Street tree planting should generally be considered at a rate of one tree per lot, with wider spacing (approximately 30 metres) along collector roads, subject to coordination with utilities and site conditions.	The text has been revised to encourage one tree per lot.
<b>Tree Trust TBM Betty Muise</b>	47	Tree Trust supports the comprehensive integration of measures to ensure tree protection and tree planting in the Community Design Guidelines. This includes measures in public spaces such as parks, open spaces, trails and waterfront areas (Section 2), as well as neighbourhoods (Section 3), commercial and employment areas (Sections 4 and 5).	Noted.
	48	We support the reference to the Town's Municipal Tree Preservation By-law. The current By-law, from 2010, is severely outdated and in need of renewal to incorporate current best practices and respond to public demands for improved tree canopy protection/enhancement regulations.	Noted.
<b>Pamela Spence</b>	<b>Volume 1</b>		
	49	Page 7 – Definitions should come from the Town's new and almost improved Official Plan (OP).	The definitions referenced for character, compatible and enhance reflect what is included in the Official Plan.  Sense of place is not referenced in the Official Plan but has been added to provide additional context for the Community Design Guidelines.
	50	Page 9 – "appropriate" is vague; buffer has been defined in OP as 30m; designate woodlots as Open Space (OS).	The Official Plan and other technical documents set the standard that is to be relied on. These documents are to be referred to collectively.
	51	Page 10 – Guideline #1 should be to maintain 30m setback as buffer from top of each bank to conform to OP. Guideline #4 should remove preferably (too weak; guideline after all is weak not law)	The Official Plan and other technical documents set the standard that is to be relied on. These documents are to be referred to collectively.
	52	Trees and Hedge Rows TBM Municipal Tree By-law only pertains to trees on public lands and does not apply to private developments.	Wording has been revised to remove reference to "all development" for clarity.
	53	Reference should be made to Town's Engineering Standards and generally accepted principals (which ever are the stronger). Specifically Standard Sec. 4.14.3 (pg 115/142) says "developer must preserve all trees within the limits of the development in accordance with the inventory assessment preservation plan/report." AND Sec. 4.14.5 "Additional trees may be required to buffer private and public areas as determined by the Town."	There is language points to this document that it should be reviewed alongside the Guidelines.
	54	Definition of a "mature tree" and measure method should be included as well as define a measure or formula to setback for the mature tree	The Guidelines include reference to the Town's tree standards, which should be reviewed in conjunction with this document to ensure consistency in interpretation. In addition, the Grey County Tree

		Inventory and Preservation Plan Technical Guidelines provide direction regarding the identification and protection of trees through the development process. These documents provide the framework for considering tree protection and preservation matters as part of development review.
55	Page 14 – Guideline #1 should be to maintain 30m setback from high water or wave uprush mark of waterfront. Native plantings should be defined or lists included as appendix.	The Official Plan and other technical documents set the standard that is to be relied on. County of grey ecologist will review individual applications on a site by site basis and include recommendations on native tree species.
56	Page 17 – Establishment of corridor along Nipissing Ridge; Town has declined to take charge of that at Parkbridge and has delayed pressing developer to do the work on its behalf. Joining of trails onto Georgian Trail should not be at 90 degree angle but by 45o entry ramp.	Noted. Nippising Ridge is outside of project scope. Georgian trail connections to be reviewed as part of parks and trail master plan
57	Page 19- definition of SWM ponds should be provided and design best practices. Some have rip rap edges which should have design guidelines to soften and integrate. Guidelines should be included re regular maintenance (per NVCA SWM pond planting guide within 10 year ponds fill in 50%) including reference to Sediment and Erosion Control Plan (Sect 3.4.11.4) and Landscape Plans for SWM Ponds (Sect. 4.5.6.5) and specify in CDG that 1 tree planted at least every 50m.	SWM Ponds to be designed in accordance with engineering best practices together with landscape plans to ensure naturalization of these facilities consistent with Community Design Guidelines.
58	Page 26 – Craigleith Depot picture is not current picture therefore replace with 2026 version.	Noted. The image has been replaced.
59	Craigleith Community Centre as an old school house – is it designated and properly maintained? How can you ask others to abide when Town does not? D5 in Town’s OP has 8 policies that should be incorporated in CDG!	Craigleith Community Centre is designated and maintained by the Town.
60	Lighting reference should be taken from the OP that requires downlighting only.	The Community Design Guidelines build upon and are aligned with the Official Plan. The lighting guidelines promote responsible lighting practices that direct illumination to intended areas while minimizing glare and off-site impacts.
61	Views and Vistas are an essential chapter and design policy in the OP and should be outlined as a separate and strong direction in CDG.	Views and vistas are referenced throughout the Guidelines.
62	Page 28 – Projects exist in TBM where the built form of roads and infrastructure (which includes parking and stormwater management ponds) have justified removal of trees in preservation plan and compromised other standards or policies and of course guidelines. It should be clarified in Section 3 that neighbourhood and built form designs should be done without compromising the policies and guidelines even though some areas permit. Through thoughtful design and rework, the guidelines can be followed.	Noted.
63	Page 29 - #7 and #8 seem to conflict with each other.	The exact wording has been revised to provide clarity.
64	Page 31 – Residential Built Form does not list rental apartment blocks prominently enough. This is most required housing form so would ask it to be first in descriptor section.	Rental refers to a tenure not a built form. The same guidelines would apply for built form.
65	Page 32 – references only 2.5 stories but OP has a height limit which should also be specified as BOTH apply in any application! Add metric heigh measures where ALL storey measures are provided.	Metres have been added for clarity
66	Where driveway guidelines are provided, minimum lengths should also be provided.	This is a zoning matter.
67	Page 36 #19 states “below grade units are discouraged”. This is the prominent type of ADU the Town is promoting and approving – this conflicts with Town policies.	Noted. The discouraging language has been removed.
68	Reference the Town’s Zoning By-law for rear/side/front yard setbacks	Reference to the specific standards of the Zoning By-law has been added to the introduction of Section 3.2

69	Page 43 references Section 6.2 but no such section!?????	The reference has been revised to Section 4.2
70	Not sure 5 storeys are permitted on Hwy 26 in Thornbury. All storey references should have maximum height in metres along with storeys.	The height has been revised to 4 storeys.  Metres have been added for clarity.
71	Page 57 does not give guidelines for Craigleith Village but should. This area is yet to be designed so critical to provide guidelines for this “evolving” area to guide staff and designers. Craigleith Village be included in multiple sub section of Section 4.	Both the Community Design Guidelines as well as the design guidelines in the Official Plan apply to Craigleith village. A lot of the guidance is best practice and would apply in this scenario.
<b>Volume 2</b>		
72	Volume 2 should be Volume 1. This document sets out the origination of and principles behind the CDG Volume 2 and should be read ahead of the guidelines. Furthermore, the Community Design Principles should be in the forethought of all design efforts and application of the Guidelines. This reinforces the need that this document be Vol 1.	Volume 1 is intended to be the primary reference for the review and preparation of development applications, and Volume 2 provides supporting context to inform how the Guidelines are applied.
73	Secondly, I read in some places that the word “urban” was used in various contexts describing the Town. I would suggest that the community has been careful and dare I say vigilant to not imply TBM is an urban community nor does it have urban areas. Please remove the word “urban” in this volume.	References to urban have been removed, other than the Urban Employment Area as this is the name of the Town’s designation.
74	Generally I think the principles are valuable and clear except for a few suggestions outlined below. Page 7 – Natural Character Design Principles should more prominently speak to preservation of views and vistas (rather than “as well as”) as this is a strong principle in the Town’s OP (D5.5). Ideally Views and Vistas would be a standout principle. – Cultural Heritage The image is correctly the current state of the Depot and is different from the Depot picture in other volume which is outdated.	The intent of this principle surrounds “visual quality”. “As well as” has been replaced with “including” for clarity.  Thank you. The previous image has been replaced.
75	Page 8 – Tourism and Recreation Remove the word “resort” ahead of community in the first Design Principle. Only part of Craigleith is a resort area and Craigleith is only part of the whole Town. The recreation and tourism aspect is sufficiently strong that inserting “resort” to Town’s total descriptor is not applicable.	This change has been made.
76	Page 21 – Craigleith “Settlement” should NOT be included in Residential Recreational designation.	Noted. And the designation has been updated
77	Page 23 – Under “broad range of housing types” priority or emphasis should be on rental stock!	Noted. Types “and tenures” has been added.
78	Page 26 – Technically Lora Bay is a Residential/Recreational and Recreational Commercial designation and settlement is not used in its context. Please remove “settlement”. (notable that on page 27 re Camperdown settlement is correctly NOT included in the descriptor!)	Noted. And the designation has been updated
79	Furthermore, I urge the Town to incorporate some LID measures and energy efficient building design suggestions in the CDG as I understand the Town's engineering dept. has been unable to decide on which could be incorporated as Town "standards". BY suggesting some in the guidelines it would be promoted that the Town is looking to see progressive designs and drainage solutions that positively influence our community.	Noted. Additional reference to resilient and energy efficient building design, as well as LIDs has been added in the document.

# The Blue Mountains – Community Design Guidelines Checklist

## Volume 1: Guidelines

The following checklist is to be completed for the proposed development, as applicable. For each guideline, indicate whether the proposed development meets the metric by selecting Yes, No, or N/A, and provide comments where appropriate.

Compliance Status Complies/ Partially Complies/ Does Not Comply/ Not Applicable	Guideline Requirement	Description of How the Guideline Direction Is Addressed (Provide drawing reference(s) as applicable)
<b>Natural Environment (Refer to Volume 1 Section 2)</b>		
	Does the development incorporate existing natural features, including woodlands, watercourses, significant trees, hedgerows, vegetation slopes and/or natural open spaces into the overall site, street and/or block design?	
	Are natural features, parks and/or open spaces framed by single-loaded streets or other design approaches that support visibility, safety and access where appropriate.	
	Are existing healthy trees proposed to be protected and retained by sensitive site and building design?	
	Are native, non-invasive and climate-resilient plantings proposed to support biodiversity, habitat function and natural character?	
	Does the development provide sufficient landscaped open space to support stormwater infiltration, retention and runoff management?	
<b>Neighbourhood Design (Refer to Volume 1 Section 3.1)</b>		
	Are streets and blocks designed to support walkability, connectivity and access to parks, trails, open spaces and/or community amenities?	
	<p>Are development blocks along Local Roads designed with a maximum length of 250 metres to support walkability, connectivity and pedestrian access.</p> <p>Where development blocks exceed 250 metres along Collector Roads or Highway 26, are mid-block pedestrian</p>	

## The Blue Mountains – Community Design Guidelines Checklist

	connections or walkways provided to improve pedestrian access?	
	Does the proposed development incorporate a mix of lot sizes, housing forms and building types?	
	Does the development incorporate site and building design measures that support passive solar design, natural ventilation, tree preservation, street tree planting and/or low impact stormwater management?	
	If provided, are shared private outdoor amenity areas designed as visible, central, accessible and functional focal points for residents?	
<b>Single Detached, Semi-Detached and Townhouse Dwellings (Refer to Volume 1 Section 3.2)</b>		
	Is the dwelling designed with a primary façade and front door that face and is accessible directly to the public sidewalk?	
	Is the building designed to provide variation in elevation, articulation, materials, colour, rooflines, windows and architectural details while maintaining compatibility with its surroundings?	
	For corner buildings, are both street-facing façades designed with a consistent level of architectural quality, including coordinated materials and enhanced architectural treatment?	
	Does the design of the garage and/or driveway reduce visual prominence from the street?	
	Are rooflines, upper storeys, garages, driveways and parking areas designed to reduce visual impacts and provide appropriate transition to adjacent dwellings?	
<b>Low-Rise Residential Buildings (Refer to Volume 1 Section 3.2)</b>		
	Is the building designed to be compatible with adjacent buildings and its surroundings through appropriate height, massing, building setbacks, roof form and façade design?	
	Is the building designed to be less than 50 metres in length? Are longer buildings	

## The Blue Mountains – Community Design Guidelines Checklist

	broken up physically or visually using stepbacks, colour, material variations and unique building articulation?	
	Does the building frame and animate the street, with the front façade and primary entrances oriented to the public realm?	
	Does the building provide appropriate setbacks, stepbacks and transitions to minimize impacts such as shadowing, overlook and wind effects on surrounding properties?	
	Are mechanical equipment, waste storage, utilities and servicing areas integrated or screened from public view?	
	Are bicycle racks, pedestrian routes, lay-by parking and drop-off areas located to support safe, convenient and accessible pedestrian movement?	
	Are parking areas located away from the street edge, where feasible, and designed with landscaping, shade trees, clear pedestrian routes, and lighting?	
	Are Electric Vehicle (EV) charging, car share or carpool spaces provided, where feasible?	
<b>Commercial and Mixed Use Development (Refer to Volume 1 Section 4.2 and 4.3)</b>		
	Is the building oriented to the street with the front façade, main entrances and active uses facing and directly relating to the public realm?	
	Is the building height, massing, scale and articulation appropriate for the surrounding context?	
	Are entrances, storefronts, windows and openings visible, accessible, grade-related and designed to support an active pedestrian environment?	
	Are materials, colours, signage, canopies, awnings and architectural details coordinated to create a high-quality and cohesive building design?	
	Is landscaping used to enhance the streetscape, frame buildings, define site edges and provide visual buffering to parking areas where applicable?	

## The Blue Mountains – Community Design Guidelines Checklist

	Are pedestrian routes clearly defined, barrier-free and safely connected to building entrances, sidewalks, parking areas and outdoor amenity spaces?	
	Are bicycle racks, pedestrian routes, lay-by parking and drop-off areas and loading/servicing areas located to support safe, convenient and accessible pedestrian movement?	
	Are parking areas designed to reduce visual impacts through location, landscaping, screening, shade trees, landscaped islands and pedestrian connections?	
	Does the site incorporate low impact development measures, sustainable landscaping practices and opportunities for stormwater infiltration where feasible?	
	Are servicing areas, waste storage, utilities and mechanical equipment integrated into the building design or appropriately screened from public view?	
	Is site lighting designed to support safety, accessibility and wayfinding while being Dark Sky Nighttime Friendly compliant?	
<b>Development in Downtown Areas (Refer to Volume 1 Section 4.2)</b>		
	Does the development reinforce the established character, scale, built form and streetscape pattern of the Downtown Area?	
	Does the building align with the established streetwall and contribute to a pedestrian-oriented public realm through storefront design, entrances, transparency and façade articulation?	
	Are building materials, signage, lighting and heritage features designed to compliment, preserve or enhance the the architectural character of the Downtown Area?	
<b>Development Along Highway 26 Through Thornbury (Refer to Volume 1 Section 4.2)</b>		
	Does the development maintain the open-space character and	

## The Blue Mountains – Community Design Guidelines Checklist

	spaciousness associated with the Highway 26 Corridor?	
	Do building setbacks incorporate landscaping, pedestrian amenities, lighting, seating, public art or other streetscape improvements, where appropriate?	
	Are taller buildings appropriately stepped back and transitioned to adjacent low-rise residential areas, where applicable?	
	Does the development contribute positively to the visual character of the Highway 26 corridor through high-quality architecture and site design?	
<b>Urban Employment Area Design (Refer to Volume 1 Section 5)</b>		
	Does the building incorporate high-quality materials, architectural detailing and building articulation along public-facing elevations?	
	Are principal building entrances clearly visible and emphasized through the building design?	
	Is the sitedesigned to incorporate landscaping, tree planting and low impact development measures, where feasible?	
	Are pedestrian routes safe, direct, accessible and clearly connected to entrances and parking areas?	
	Is bicycle parking provided in convenient and visible locations?	
	Are parking, loading, servicing and waste areas designed to minimize visual impacts through landscaping, building placement or screening?	
	Is site lighting designed to support safety, accessibility and wayfinding while being Dark Sky Nighttime Friendly compliant?	
<b>Public Art (Refer to Volume 1 Section 2.6)</b>		
	Does the proposal incorporate opportunities for public art that contribute to the character, identity and visual interest of the site and surrounding public realm?	

# The Blue Mountains – Community Design Guidelines Checklist

## Volume 2: Vision and Community Structure

Please select and complete the checklist that corresponds to the location of the proposed development. For each applicable design objective, indicate whether the proposed development helps implement the objective by selecting Yes, No, or N/A.

### Thornbury:

#### Yes/No/NA

Residential Neighbourhoods	
	Enhance the small-town character of existing and established residential neighbourhoods in Thornbury and Clarksburg by ensuring development and redevelopment is compatible with the scale and density of existing development.
	Encourage the development of a broad range of housing types, from single-detached and semi-detached dwellings to townhouses and low rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.
	Encourage gentle infill and intensification in neighbourhoods through well-designed additional residential units and converted dwellings.
	Create and maintain walkable neighbourhoods that are well connected to their surrounding downtown area, parks, open space and community amenities.
	Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

#### Yes/No/NA

Downtown Thornbury	
	Preserve the historic low-rise character of Thornbury's downtown core along Bruce Street.
	Ensure the scale and location of new development or redevelopment maintains and/or enhances the distinct heritage character of Downtown Thornbury.
	Reinforce the function of Downtown Thornbury as a primary cultural, business, entertainment and commercial focal point.
	Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.
	Encourage coordinated streetscape and façade improvements geared to the pedestrian that revitalize the cultural and historic character of Thornbury.
	Preserve and enhance cultural and historic features that exist within Downtown Thornbury, including the restoration and enhancement of building facades.
	Create new community focal points in Downtown Thornbury by encouraging the creation of new public spaces, pedestrian connections and public art.
	Enhance the pedestrian experience and connections along Bruce Street from the downtown core to Thornbury Harbour and the Georgian Bay waterfront.

## The Blue Mountains – Community Design Guidelines Checklist

Yes/No/NA

<b>Downtown Clarksburg</b>	
	Reinforce the vibrancy of Downtown Clarksburg as a local, vibrant community with a strong sense of place tied to arts and culture.
	Ensure the scale and location of new development or redevelopment maintains or enhances the built form and streetscape character of Downtown Clarksburg.
	Reinforce the function of Downtown Clarksburg as a mixed-use focal point for the surrounding community, accommodating cultural, business, entertainment, commercial and service uses.
	Encourage a diverse, broad mix of compatible uses, including residential units above commercial uses, to enhance the overall vibrancy and accessibility of amenities within downtown.
	Encourage coordinated streetscape and façade improvements geared to the pedestrian that build on the sense of place for Clarksburg.
	Preserve and enhance cultural and historic features that exist within Downtown Clarksburg, including the restoration and enhancement of building facades.
	Create new community focal points in Downtown Clarksburg by encouraging the creation of new public spaces, pedestrian connections and public art.

Yes/No/NA

<b>Highway 26 Through Thornbury</b>	
	Establish Highway 26 through Thornbury as a priority area for mixed-use development, redevelopment and intensification.
	Ensure development fronting onto Highway 26 is held to a high standard to building, landscape and site design, as the main gateway to and from Thornbury.
	Encourage the development of a broad mix of uses, including residential, to provide access to range of amenities, as well as vibrant and safe public places within walking distance.
	Ensure the low-rise character of adjacent neighbourhoods and Downtown Thornbury is protected through compatible intensification and appropriate transition.
	Maintain and enhance the open space character of properties along Highway 26 and ensure the continued and improved feeling of spaciousness through development and redevelopment.

Yes/No/NA

<b>Urban Employment Area</b>	
	Prioritize opportunities for a diverse range of existing and new businesses to grow and develop in Thornbury in strategic locations.
	Ensure employment area development fronting onto the public realm is held to a high standard of building, landscape and site design.
	Ensure employment and industrial uses are adequately buffered and screened from any adjacent residential, open space or recreational use.

**Craigeleith**

## The Blue Mountains – Community Design Guidelines Checklist

Yes/No/NA

Residential Neighbourhoods	
	Enhance the character of existing residential neighbourhoods in Craigleith by ensuring development and redevelopment is compatible with the scale and density of existing development.
	Encourage the development of a broad range of housing types and tenures, from single-detached and semi-detached dwellings to townhouses and low-rise apartments, in appropriate locations while maintaining compatibility with existing adjacent character.
	Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
	Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space and community amenities.
	Prioritize pedestrian-oriented design, cycling facilities and trail connections throughout Craigleith.
	Reinforce the uniqueness of each neighbourhood, with a focus on distinct heritage characteristics, civic and gathering spaces, natural features and areas, recreation amenities and accessibility.
	Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

Yes/No/NA

Craigleith Village	
	Develop a strong identity for Craigleith Village that accommodates a mix of residential, commercial and institutional uses and creates a new central node for residents and visitors.
	Ensure the scale and pattern of development in Craigleith Village respects surrounding residential lands and provides an appropriate transition.
	Encourage redevelopment in Craigleith Village to maximize residential intensification opportunities and variety through building sizes and inclusion of residential units above commercial uses.
	Encourage new coordinated streetscapes geared to the pedestrian, as well as new community focal points including public squares, that create a unique sense of place for Craigleith Village.
	Ensure the community road and trail network provide residents with a safe, functional and attractive neighbourhood that offers access to sustainable natural areas including wetlands, tree cover and the shoreline.
	Encourage the development of new recreation opportunities are tied to the environment and active, healthy living.

### Lora Bay

Yes/No/NA

Residential Neighbourhoods	
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## The Blue Mountains – Community Design Guidelines Checklist

	Encourage development that is in keeping with the evolving open space and built form character of Lora Bay.
	Encourage a mix a broad range of housing types in appropriate locations while maintaining compatibility with existing adjacent character.
	Encourage gentle infill in neighbourhoods through well-designed additional residential units.
	Ensure neighbourhoods are well connected to parks, open space, the waterfront and community amenities.
	Ensure new residential development reinforces the uniqueness of each neighbourhood, including the prominent lot pattern, landscape and open space character.

### Camperdown

Yes/No/NA

<b>Residential Neighbourhoods</b>	
	Ensure new residential development reinforces the uniqueness of each neighbourhood area in Camperdown, including the prominent lot pattern, scale, built form characteristics, landscape and open space character.
	Encourage a broad range of housing types while maintaining compatibility with existing character.
	Encourage infilling and additional residential units in appropriate locations through well-designed residential built form to support walkability and create well-rounded neighbourhoods.
	Create and maintain walkable, compact neighbourhoods that are well connected to other residential areas, commercial uses, parks, open space, the waterfront and community amenities.
	Prioritize pedestrian-oriented design, cycling facilities and trail connections.

### Villages and Hamlets

Yes/No/NA

	Ensure any new development or redevelopment is compatible with the character, role, scale and function of the surrounding natural, agricultural and rural landscape.
	Ensure Hamlets are maintained as focal points for the surrounding rural community and tourism with a mix of diverse and compatible uses in proximity to each other.
	Protect and enhance the natural character of the Niagara Escarpment as a unique and important feature of the Town.
	Carefully control new residential development in Hamlet Areas in order to maintain existing scale and character.
	Provide opportunities for small-scale commercial and tourism related uses that are compatible with the character and scale of these areas.

# The Blue Mountains – Community Design Guidelines Checklist

	Preserve and enhance the natural and historic features that exist in each rural community.
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